

SeaCert

Seafarer Certification and
Operational Limits Framework

2 July 2014



This document sets out the seafarer certification and operational limits framework for New Zealand. It supplements the advisory circulars for Maritime Rule Part 20 (Operational Limits), Part 31 (Crewing and Watchkeeping), Part 32 (Seafarer Certification), Part 34 (Medical Standards) and Part 35 (Training and Examination).

Disclaimer: Maritime New Zealand (MNZ) guidance notes provide supporting information about specific technical issues or areas related to maritime rules. This information should not be treated as a substitute for the rules, which are the law.

More information about SeaCert is available on MNZ's website:

maritimenz.govt.nz/seafarers

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Overview

Note: Please check MNZ's website to ensure that you are referring to the most recent version of this document: maritimenz.govt.nz

SeaCert is Maritime New Zealand's new framework for seafarer certificates and operational limits. Certificates of competency or proficiency are required for all crew employed in positions of responsibility on board commercial vessels, along with certificates of proficiency for some support positions. Operational limits clearly define the boundaries within which the certificates can be used.

The measurement of competence is central to SeaCert – you must demonstrate competency in order to gain, maintain and advance certificates. Each certificate clearly describes its privileges – what you can do and where you can go.

The changes introduced by SeaCert have the greatest impact on seafarers working within restricted limits (enclosed and inshore waters). It is within these limits that the biggest number and variety of commercial operations take place, and it is here that SeaCert provides positive changes that will facilitate entry to the commercial maritime industry and provide clear career progression. SeaCert also takes account of international standards and allows for portability of qualifications between countries.

Development of SeaCert

Maritime New Zealand (MNZ) recognised that the existing qualifications and operational limits framework was outdated, confusing and difficult to administer. It began a fundamental review of maritime qualifications and their associated operational limits – the QOL Review – in 2009.

The starting point for this review was a community engagement programme. MNZ held 17 public meetings throughout New Zealand, attended by 434 people from across all maritime sectors. These meetings gathered in-depth information about the problems that existed with qualifications and operational limits. A report summarising the issues was released in February 2010.

MNZ discussed possible solutions to the issues with a wide range of industry groups and individuals, and released the QOL Framework document in September 2011 as a basis for further consultation. This consultation was carried out in 2011 at 17 meetings across the country, attended by 501 people.

Further amendments were then made to the QOL Framework, which was released in November 2012 as SeaCert. After further consultation, maritime rules were amended and the new SeaCert framework came into force in early 2014. Further information about the development of SeaCert can be found on MNZ's website: maritimenz.govt.nz/seacert.

Key principles

The following key principles for seafarer certification were applied to the design of SeaCert:

- Retain the quality of New Zealand certificates
- Demonstrate competency and knowledge
- Emphasise competency required within an operating limit
- Manage the risks of carrying large numbers of passengers by requiring specialist knowledge
- Manage the risks of high-speed navigation by requiring specialist training and experience
- Use consistent vessel length and tonnage parameters
- Use consistent entry-level models for deck, fishing and engineering certificates
- Enable necessary specialist skills to be recognised through endorsements
- Require local area knowledge to be demonstrated, where appropriate
- Allow seafarers to gain the experience necessary for a higher certificate within an operational limit

- Recognise prior learning
- Align with the international standards of STCW (International Convention on Standards of Training, Certification and Watchkeeping for Seafarers, 1978, as amended in 2010) and STCW-F (International Convention on Standards of Training, Certification and Watchkeeping for Fishing Vessel Personnel, 1995), to enable portability of certificates.

Key changes to existing certification and operational limits

Operational limits

- Provide a new specified limit to cater for very restricted operations close to shore
- Retain existing defined inshore limits and add three new defined limits
- Remove the prohibition on assigning an inshore limit to an individual vessel for Cook Strait
- Redefine the coastal limit as a uniform 50 nautical miles (NM) from the coasts of New Zealand and Chatham Islands
- Extend the offshore limit to the outer limit of the exclusive economic zone (EEZ)
- Extend the unlimited area from the new offshore limit
- Allow port companies and local authorities to obtain extended enclosed limits.

Certification

- Greater emphasis on competence and proven ability
- Greater emphasis on practical and assessment components, supported by evidence-based task books (training record books)
- Greater emphasis on recognition of quality sea service
- Introduction of practical assessment at entry level, where applicable
- Reduced number of hours of sea service required for entry-level certificates
- Recognition of recreational sea service at entry level
- Ability to gain sea service for the next operational limit within the existing limit, where possible
- Streamlined certificates to replace Inshore Launchmaster (ILM), Local Launch Operator (LLO), Advanced Deckhand (ADH) and Certified Deckhand (CDH)
- Career progression in restricted limits through experience and endorsement of specialist skills
- Alignment of certification for fishing vessels of 24m (metres) or more operating beyond restricted limits with STCW-F
- Ability to obtain a conditional limit (temporary extension to a defined limit) for seasonal commercial operations
- Recognition of watchkeeping time aboard all warships
- Maintenance of currency through renewal at all levels
- Close oversight by MNZ of the entire examination system
- Full application of STCW (Manila) for certificates for commercial ships of 24m or more in coastal and offshore areas
- New master yacht certificate for sailing vessels of 24m or more in the near-coastal area
- Replacement of 45m limit with 500GT (gross tonnes), to align with STCW requirements
- Use of minimum safe crewing assessment to determine certification aboard vessels of between 500 and 3000GT operating exclusively within restricted limits
- Requirement for all seafarers aboard vessels of 3000GT or more operating exclusively in restricted or near coastal limits to hold unlimited certificates
- Alignment with Australian and international standards, to ensure portability
- Sea service for superyachts amended to align with the United Kingdom's Maritime and Coastguard Agency (MCA) requirements.

Part 1 Operational limits

This section summarises the operational limits that define:

- boundaries where the competencies required to operate change
- training, skills and knowledge required for specific areas
- experience required to operate within these areas.

As well as being defined by distance from the shore, in some cases operational limits are defined by distance up and down the coast. Both elements are taken into account, along with the constraints resulting from international maritime obligations.

In determining operational limits, MNZ applies the key principles that they should be:

- common for fishing and non-fishing vessels, where appropriate, to help seafarers working in both sectors with the transfer of maritime certificates
- future proofed, to better support New Zealand industry
- justified, defined and applied consistently
- STCW-compliant and align with STCW-F.

SeaCert complies with STCW and ensures New Zealand's 'white list' status (assessed as properly implementing all the requirements of STCW) is maintained. It ensures portability, so that MNZ certificate holders can operate internationally, and is also aligned with STCW-F.

The operational limits are:

- Specified limit – a permit allowing specific restricted activities very close to shore
- Enclosed – existing enclosed limit
- Inshore – existing defined inshore limits, with three additional defined limits
- Inshore fishing – within the 12NM territorial limit
- Coastal – 50NM off New Zealand and Chatham Islands
- Offshore – to the outer boundary of the EEZ
- Unlimited – beyond the EEZ.

The operational limits are illustrated in Figures 1, 2 and 3.

The following combined areas are also referred to throughout this document:

- Restricted limits – enclosed and inshore limits
- Near-coastal – coastal and offshore limits for STCW certificates
- Limited waters – coastal and offshore limits for STCW-F certificates.

Specified limit permit

The specified limit permit provides for permanent or temporary commercial operations within a restricted area very close to shore or to a structure close to the shore. The specified limit must be outside the normal flow of marine traffic.

Specified limits will not be approved in areas within the normal flow of marine traffic and typically would not be more than 200m from the shore or a structure. Voyages from one specified area to another are not permitted. Carrying fare-paying passengers is not permitted. Specified limits are limited to vessels of less than 12m.

Examples of commercial operations that could be covered by a specified limit include marina and salmon farm workboats, vessels used for wharf construction or tidal oyster farms, and yacht club safety support boats.

Within an approved specified limit (which may be multiple, but not contiguous or adjacent areas), an operator is required to have a specified limits operating plan and is able to use alternative certificates – for example, Coastguard Day Skipper or RYA Powerboat Level II – to acquire the minimum required competencies without sea service. MNZ monitors compliance, but has no other involvement apart from approving the specified limit and associated operating plan.

A specified limit permit does not replace the ability for organisations to develop their own training programmes, as allowed for under Maritime Rule Part 35.

Enclosed waters limit

The enclosed waters limit includes specific areas identified in Appendix 1 of Maritime Rule Part 20 and all New Zealand inland waters. It equates to STCW's definition of "...inland waters ... or waters within sheltered waters". STCW requirements do not apply within enclosed limits.

Individual enclosed limits are capable of being expanded to allow 'pilotage' limits for certain vessels.

Inshore limit

Changes to the inshore limits from those defined in the previous Part 20 include:

- slightly extending the Bay of Plenty limit to allow direct transits between Tauranga harbour and White Island
- extending the Foveaux limit westwards to include Codfish Island
- treating contiguous (adjacent) inshore areas as a single area for the purposes of certificates, survey, crewing and equipment
- allowing surveyors to assign an inshore limit to an individual vessel for Cook Strait (only for vessels of at least 12m and less than 500GT).

The inshore limit encompasses "waters closely adjacent to sheltered waters", as defined by STCW. STCW requirements do not apply within the inshore limit, except for vessels of 3000GT or more operating exclusively within restricted limits.

Inshore fishing limit

The inshore fishing limit comprises all the waters within New Zealand's territorial sea and the waters of the coastal limits north of a line from 36 degrees 42 minutes south, 167 degrees 08 minutes east to 36 degrees 42 minutes south, 167 degrees 18 minutes east that provide direct access to Solander Island from Foveaux Strait.

Coastal limit

The coastal limit is a uniform distance of 50NM off the coasts of North, South and Stewart Islands and 50NM off the coast of the Chatham Islands.

For New Zealand purposes, the coastal limit is within "near-coastal waters", as defined by STCW. Full STCW requirements apply to certificates required for vessels of 24m or longer within the coastal limit.

Offshore limit

The offshore limit extends from the coastal limit to the outer limit of the EEZ and is treated as the outer limit of near-coastal waters, as defined by STCW.

Full STCW requirements apply to certificates required for vessels of 24m or longer within the offshore area.

Unlimited area

The inner boundary of the unlimited area is the outer boundary of the EEZ.

Part 2 Seafarer certification

Competency-based certification

SeaCert introduces a competency-based approach to seafarer certification. This brings New Zealand's certification system in line with international best practice for seafarer training and ensuring ongoing competence.

The new Maritime Rule Part 32 requires seafarers to achieve and maintain the required levels of competency in order to be issued and retain a certificate. The competencies for STCW and STCW-F certificates are incorporated by reference to the relevant STCW or STCW-F provisions in the rule.

High-level competencies for national certificates are also set out in Part 32. These are supported by a more detailed competency framework for the national certificates, developed in consultation with industry, training providers and the industry training organisation for the maritime sector, Competenz. The framework is modelled on the STCW Competency Framework, to facilitate career progression from national to international certificates.

To receive a New Zealand certificate, a seafarer must show they have the required competencies. They gain these competencies through:

- sea service (a set amount of time served on board an appropriate vessel)
- on-board training, including completing training record books (also known as STaRS books)
- shore-based training.

The seafarer must also demonstrate their competence by undertaking and passing a final examination and/or assessment conducted by an MNZ-approved examiner or assessor.

Other requirements such as medical, eyesight and fit and proper person assessments must be met before the seafarer can be issued with a certificate. Information about these requirements can be found on MNZ's website: maritimenz.govt.nz/seafarers.

Sea service

All approved sea service must be gained within the 10 years immediately before applying for a certificate.

Generally, service on board vessels operating exclusively in restricted limits that are required to be crewed by persons holding unlimited certificates will be treated as sea service as defined by STCW. For instance, service in the deck department of a vessel that is 3000GT or more operating exclusively in restricted limits can be accepted as sea service for STCW certificates. Likewise, service in the engine department of a vessel with 3000kW or more main propulsion power operating exclusively in restricted limits can be accepted as sea service for STCW certificates.

Sea service aboard warships is recognised in full for engineering certificates. Navigational watchkeeping time aboard warships is also recognised in full.

On-board training

On-board training, in association with the required sea time for a certificate, is intended to build and embed a seafarer's competencies through practical experience. This training is recorded in a training record book approved by the MNZ Director (also known as a seafarer training and record of service or STaRS book). Training record books are evidence-based and include competencies and tasks required by the crew and skippers of restricted-limits commercial vessels, and for some STCW deck and engineer certificates.

The skipper of the vessel or another qualified crew member must confirm that the tasks have been satisfactorily carried out. If sea service has been acquired on board a non-commercial vessel, a practical assessment carried out by an MNZ assessor aboard a vessel is required. The completed training record book will be provided to the practical assessor, training provider and final examiner.

Shore-based training

Shore-based training is expected to deliver both knowledge and competency, which is confirmed through a competency-based assessment process run by training providers. The STCW, STCW-F and national competency frameworks provide the basis for the development of training courses for each national certificate or endorsement.

This training may be provided by public and private training providers, and, in some instances (such as some ancillary proficiency training), by vessel operators, through courses that have been either accepted or approved by the MNZ Director. Further information about accepted and approved courses is provided in the section on *Training and examinations*.

Examinations and practical assessments

The objective of the final examination or practical assessment is to satisfy MNZ that the seafarer has the competencies and proficiencies required for a maritime certificate or endorsement. The examination or assessment is based on the competencies and proficiencies set out in the relevant STCW, STCW-F or national competency framework, as appropriate.

Failure to pass the final examination or assessment will mean the seafarer is not issued with a certificate *even if they have passed* the required training courses and met the sea service and other requirements for the certificate or endorsement.

Practical and competency-based assessments must be undertaken on the water and carried out by experienced commercial skippers acting as MNZ assessors.

MNZ has resources dedicated to overseeing and supporting examiners and assessors working within the examination system. Examiners and assessors may be employed by a training provider and conduct internal assessments for training courses.

Domestic Passenger and
 Non-Passenger Certificates
 Vessels <24m – Coastal and Offshore

COASTAL LIMIT – 50NM

Master	SCO
	Master Yacht <24m
Mate	SRL <24m
Engineer	MEC 4 (Complex Passenger)
	MEC 5
	MEC 6 (Simple Non-Passenger)
Second Engineer	MEC 6

OFFSHORE LIMIT – EEZ

Master	SCO
	Master Yacht <24m
Mate	SCO
Engineer	MEC 4 (Passenger)
	MEC 5
Second Engineer	MEC 6

RESTRICTED LIMITS

Master	SRL
Engineer	MEC 5
	MEC 6
	QOC

Figure 1 Operational limits for domestic passenger and non-passenger certificates

Operational Limits for Fishing Vessels

COASTAL LIMIT – 50NM

Master	SFV – Limited (<45m)
Mate	MFV – Limited (45m)
Deckhand	ADH-F
Chief Engineer	MEC 4
Second Engineer	MEC 6
Coastal Vessels <24m	
Master	SCO
Engineer	MEC 4/5/6

OFFSHORE LIMIT – EEZ

Master	SFV – Limited (<45m)
Mate	MFV – Limited (<45m)
Deckhand	ADH-F
Chief Engineer	MEC 4
Second Engineer	MEC 5/6
Offshore Vessels <24m	
Master	SCO
Mate	SCO (mate only required if beyond 100NM)
Engineer	MEC 4 (MEC 5/6 if <750kW)

INSHORE FISHING LIMIT

Master	SRL with appropriate endorsement
Engineer	MEC 4 (MEC 5 if <3000kW, MEC 6 if <750kW)

UNLIMITED

Master	SFV – Unlimited
Mate	MFV – Unlimited
Deckhand	ADH-F
Chief Engineer	MEC 2 (MEC 4 if <3000kW)
Second Engineer	MEC 5 (MEC 6 if <750kW)

Figure 2 Operational limits for fishing vessels

Operational Limits – STCW Vessel Certificates

COASTAL AND OFFSHORE LIMITS

Master	Master Master <3000GT Master <500GT Near Coastal
Mate	Chief Mate Chief Mate <3000GT
OOW	Watchkeeper Deck Watchkeeper Deck <500GT
Chief Engineer	MEC 1 MEC 2 ECE (<3000kW vessels) MEC 4 (<750kW vessels)
Second Engineer	MEC 2 MEC 2 <3000kW MEC 6 (<750kW vessels)
OOW Engineer	MEC 3

RESTRICTED LIMITS

SUPERYACHT CERTIFICATES – ALL LIMITS

Master Yacht
 Master Yacht <500GT
 Chief Mate Yacht

OTHER CERTIFICATES – ALL LIMITS

Deck Watch Rating
 Engine Room Watch Rating
 Able Seafarer Deck
 Able Seafarer Engine
 Integrated Rating
 Electro-Technical Rating
 Electro-Technical Officer

UNLIMITED

Master	Master Master <3000GT
Mate	Chief Mate Chief Mate <3000GT
OOW	Watchkeeper Deck
Chief Engineer	MEC 1 MEC 2 ECE
Second Engineer	MEC 2 MEC 2 <3000kW
OOW Engineer	MEC 3

Figure 3 Operational limits for STCW certificates

New Zealand seafarer certificates and endorsements

Figure 2 presents a high-level view of SeaCert, showing the progression of certificates within each operational limit. The more detailed diagrams that follow show the sea service and other requirements to progress to each certificate.

Not all information can be shown in the diagrams. Refer instead to the detailed table of requirements for each certificate and/or to the maritime rules on MNZ's website: maritimenz.govt.nz/seafarers

The number with each certificate in the diagrams corresponds with the number of the summary provided for that certificate later in this section.

The diagrams show a vertical progression through the certificate structure. For information about non-traditional career progression, refer to MNZ's website.

The certificate summaries provide the following information:

- **Short name:** the abbreviation of the full certificate name used in this document
- **Replaces:** the certificate(s) under previous maritime rules that the certificate replaces
- **Regulation:** the STCW or STCW-F regulation reference, where relevant
- **Operational limit:** the greatest operating limit in which the certificate is valid
- **Prior certification:** what, if any, qualification is required
- **Privileges:** the highest privilege that applies to the certificate
- **Minimum age:** the minimum age limit for certificate applicants, calculated by taking into account minimum age requirements for prerequisite certificates and the age the person must be in practice, having completed any required sea time. Sea service may be gained prior to reaching this age, subject to a minimum age of 15 years for all sea service
- **Minimum service:** the minimum sea service requirements acceptable to the Director of MNZ
- **Training and supplementary certificates:** prerequisite certificates, supplementary certificates and safety training requirements
- **Career progression:** the certificates or endorsements to which a candidate could progress.

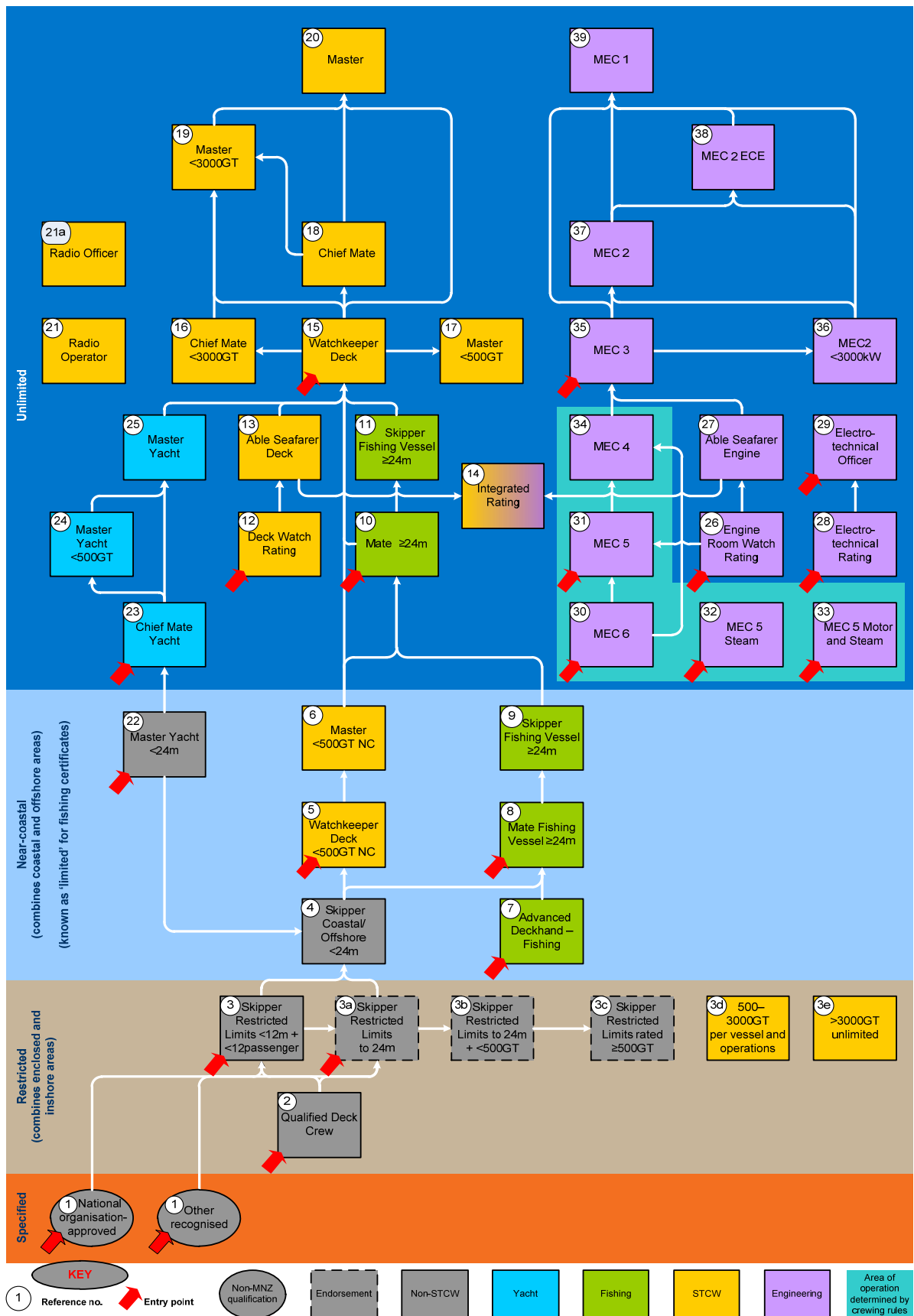


Figure 4 Progression pathways for New Zealand certificates

Certificates for specified, enclosed and inshore limits

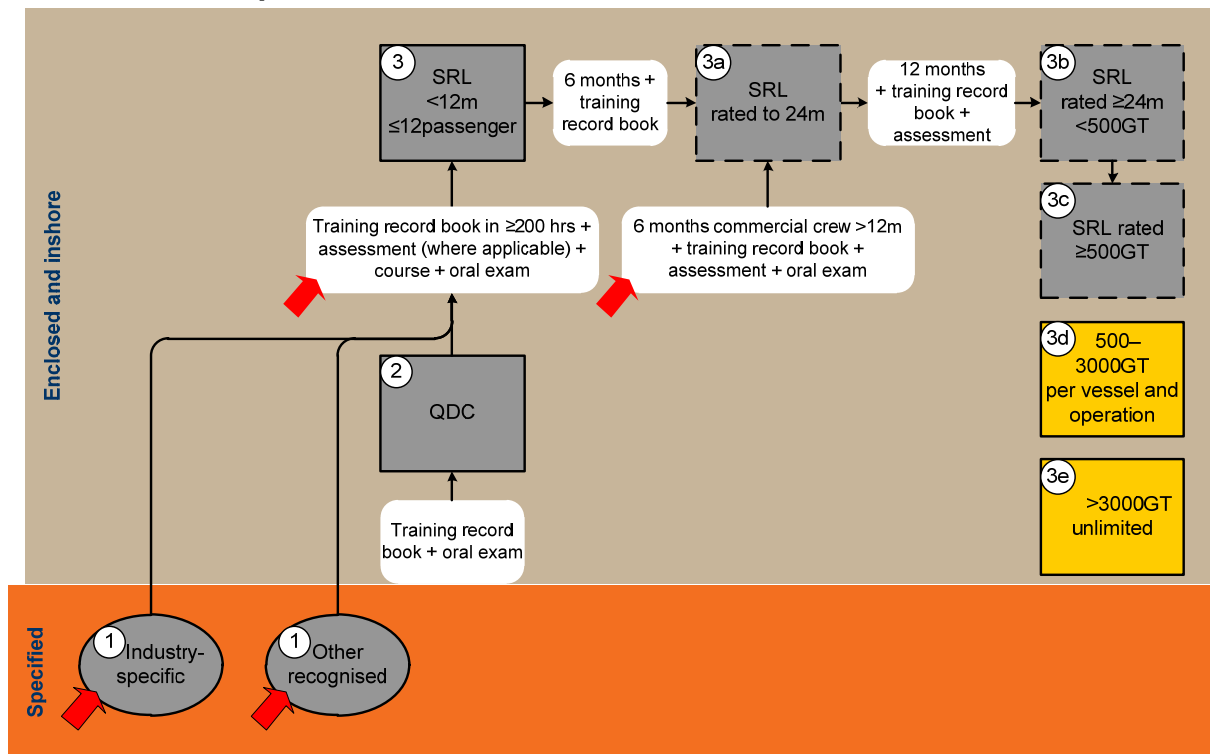


Figure 5 Certificates associated with specified, enclosed and inshore limits

1 Specified limit permits

The specified limit permit recognises commercial operations where the skipper's main activities are not within the maritime industry and they are unlikely to be able to achieve the sea service needed for a certificate. Most operators in this category are not seeking a career path in the maritime industry.

The applicant is required to have a specified limits operating plan and, with the Director of MNZ's discretion, may be able to use alternative certificates or training, such as Coastguard Day Skipper or RYA Powerboat Level II, to acquire the minimum required competencies without sea service. MNZ monitors compliance but has no other involvement apart from approving the specified limit and associated operating plan.

The specified limit permit does not replace the ability for organisations to develop their own training programmes, as allowed for under Part 35 of the maritime rules.

Privileges

- Operations in very restricted areas close to shore or to a structure
- Any vessel up to 12m
- May carry staff not operating as crew
- Multiple specified areas may be approved, but must not be contiguous (adjacent).

Exclusions

- May not undertake voyages or operate in areas within normal flows of marine traffic
- No transits allowed between specified areas
- May not be used for:
 - crossing harbours
 - delivering workers to site
 - travelling to a specified area from a port.

Training and experience required

- National bodies may have internal training programmes approved by MNZ under Part 35
- Certificates such as day skipper or RYA powerboat Level II are acceptable for organisations or individuals using a specified limit.

Examples of application

- Marina workboats
- Tidal oyster farm workboats
- Club powerboats following rowing or sailing craft
- Water sampling for councils
- Sewerage settling ponds
- Salmon farm workboats
- Boats used onsite for construction projects such as a pipeline or jetty.

2 Qualified Deck Crew

Where crewing rules or minimum safe crewing assessments require a qualified deck crew member to be aboard a passenger or non-passenger vessel, that position is filled by the holder of a Qualified Deck Crew certificate.

On vessels carrying more than 250 passengers in restricted limits, a crew member is required to hold a passenger endorsement – a QDC holder with a passenger endorsement fulfils this requirement.

Short name	QDC
Replaces	CDH and ADH
Operational limit	<ul style="list-style-type: none">• Enclosed and inshore• Coastal and offshore on vessels less than 24m length
Privileges	Deck crew member on passenger and non-passenger ships of less than 24m in length in restricted, coastal and offshore limits
Prior certification	None
Minimum age	16
Minimum service	Conditions: training record book must be completed aboard a commercial ship or warship
Training and supplementary certificates	<ul style="list-style-type: none">• Completion of an approved training record book• Current first aid certificate• MROC
Career progression	To SRL, after completing all tasks in the QDC training record book, gaining at least 200 hours' sea service, completing the tasks for the SRL training record book and passing an approved course

3 Skipper Restricted Limits

Skipper Restricted Limits is an entry-level command certificate, structured to allow entry into the maritime industry and career progression in restricted limits. This is achieved by gaining experience and specialist knowledge through endorsements, where necessary.

Short name	SRL
Replaces	ILM and LLO
Operational limit	Enclosed, and inshore and inshore fishing limit
Privileges	<ul style="list-style-type: none"> • Skipper of a passenger/non-passenger vessel operating in enclosed and inshore limits • Skipper of a fishing vessel in restricted limits and inshore fishing limit • Vessels less than 12m length overall • May carry 12 or fewer passengers
Prior certification	None
Minimum age	18
Minimum service	<p><i>Duration:</i> 200 hours</p> <p><i>Vessel type:</i> any powered vessel, including pleasure craft</p> <p><i>Conditions:</i> sea service on a pleasure craft must be as skipper</p>
Training and supplementary certificates	<ul style="list-style-type: none"> • Completion of an approved training record book¹ • Practical assessment if sea service is on a pleasure craft • Completion of an accepted/approved training course • Current first aid certificate • MROC
Career progression	<p>An endorsement may be obtained to command vessels of 12–24m after six months' sea service while holding SRL</p> <p>A passenger endorsement may be gained to carry more than 12 passengers after six months' sea service as SRL while holding SRL (other endorsements such as high-speed or square-rigged sailing may be required)</p>

¹ The training record book includes training and demonstrating competence in the fire fighting and survival craft ancillary proficiencies set out in the schedule to Part 32.

3a Skipper Restricted Limits endorsed to 24 metres

This endorsement enables entry into the enclosed and inshore limits directly as a skipper, or by advancing from the SRL certificate. It recognises that some operators may wish to enter the industry directly on a vessel of 12m or more.

Short name	SRL <24m
Replaces	ILM, LLO and NZOW with ILM endorsement for vessels carrying more than 50 passengers in inshore limits
Operational limit	Enclosed, and inshore and inshore fishing limit
Privileges	<ul style="list-style-type: none"> • Skipper of a vessel operating in enclosed and inshore limits, and the inshore fishing limit • Vessels less than 24m length overall • May carry 12 or fewer passengers • Passenger endorsement required to carry 12 or more passengers • Other endorsements as required by the nature of the operation (such as high-speed or square-rigged)
Prior certification	SRL
Minimum age	18
Minimum service	<p>Entry as skipper with SRL certificate</p> <p><i>Duration:</i> six months while holding SRL certificate</p> <p>Direct entry without SRL certificate</p> <p><i>Duration:</i> six months</p> <p><i>Vessel length:</i> 12m or more overall</p> <p><i>Role:</i> crew member aboard commercial ship</p> <p><i>Conditions:</i> Approved training record book completed</p>
Training and supplementary certificates	<p>Entry as skipper with SRL certificate</p> <p>Record of six months' sea service</p> <p>Direct entry without SRL certificate</p> <p>Completion of SRL-approved training record book,² accepted/approved training course, supplementary certificates for SRL and passing an SRL final exam</p>
Career progression	<p>Endorsement for restricted-limits vessels up to 500GT after 12 months' sea service on vessels of 12–24m while holding SRL <24m, completion of training record book for SRL <500GT endorsement and assessment by an MNZ assessor</p> <p>To SCO, with 12 months' sea service in any limit as SRL<24m, an accepted/approved training course and examination</p>

² The training record book includes training and demonstrating competence in the fire fighting and survival craft ancillary proficiencies set out in the schedule to Part 32.

3b Skipper Restricted Limits endorsed to less than 500 gross tonnes

This endorsement recognises the added competency and complexity such as stability, tonnage and load line issues associated with operating a vessel of 24m or more.

It also establishes a career path for seafarers to operate considerably larger vessels within restricted limits, recognising that not everyone wishes to advance to operating limits further from the coast.

Short name	SRL <500GT
Replaces	LLO, ILM and NZ offshore watchkeeper (NZOW) with ILM endorsement for vessels carrying more than 50 passengers in inshore limits
Operational limit	Enclosed, and inshore and inshore fishing limit
Privileges	<ul style="list-style-type: none">• Skipper of vessel operating exclusively in enclosed and inshore limits, and inshore fishing limit• Vessels of 24m or more overall that are less than 500GT
Prior certification	SRL endorsed to 24m
Minimum age	19
Minimum service	<i>Duration:</i> 12 months <i>Vessel length:</i> 12m or more length overall <i>While holding:</i> SRL certificate endorsed to 24m
Training and supplementary certificates	<ul style="list-style-type: none">• Approved training record book completed• Practical assessment by MNZ assessor
Career progression	For vessels of 500–3000GT operating exclusively in restricted limits, a higher certificate or endorsement is required (see 3c below) See Figure 4 for pathways to Watchkeeper Deck <500GT Near-Coastal and Mate Fishing Vessel – Limited

3c & 3d Vessels of 500–3000 gross tonnes operating exclusively in restricted limits

The certificates required for crewing aboard vessels of 500–3000GT operating exclusively in restricted limits depend on the size and nature of the vessel and its operation, as determined by a minimum safe crewing assessment. The certificates may vary – for example, an SRL certificate endorsed for a specified ship or class of ship of 500GT or more, or near-coastal or unlimited certificates for larger ships.

Part 32.63 provides that the Director of MNZ must endorse a certificate of competency as SRL to 500–3000GT if satisfied that the applicant:

- holds a certificate as SRL endorsed to 500GT or equivalent
- has successfully completed a training course or structured training programme that the Director has approved as recognised seafarer training for the endorsement
- has passed a relevant assessment.

3e Vessels of 3000 gross tonnes or more operating exclusively in restricted limits

Vessels of 3000GT or more operating exclusively in restricted limits are required to be crewed by holders of STCW unlimited certificates.

Endorsements

The maritime rules make provision for certificates to be endorsed as evidence that the holder has the experience required to operate larger vessels or the knowledge required for specialist responsibilities or vessel types.

High-speed endorsement

A high-speed endorsement may be required under a vessel's minimum safe crewing document (MSCD) for masters and officers to operate vessels at 25 knots or more that are:

- 20–35m in length
- 35m or longer and do not proceed beyond restricted limits
- carrying more than 50 passengers.

A high-speed endorsement may also be required under a MSCD to operate vessels that are:

- less than 20m in length
- capable of speeds of 30 knots or more
- carrying more than 12 passengers
- intended to be used for high-speed passenger rides.

Passenger endorsement

A passenger endorsement is required to SRL and SCO certificates if more than 12 passengers are carried, and to a QDC certificate if more than 250 passengers are carried.

The holder of an SRL certificate is eligible to undertake a training course for a passenger endorsement after gaining six months' sea service while holding the SRL certificate. The holder of an SCO certificate is also required to hold a passenger endorsement if carrying more than 12 passengers.

To gain a passenger endorsement, a course must be completed similar to that required by STCW Section A-V/2, for crowd management training, safety training for personnel providing direct service to passengers in passenger spaces, and crisis management and human behaviour training.

Square-rigged endorsement

Masters and some officers of square-rigged sailing ships are required to have a square-rigged endorsement. Existing industry standards, such as the Nautical Institute's international sail endorsement, can be used to obtain this endorsement, along with the sea service required by Part 32.65.

Temporary seasonal endorsement on SRL certificate

Part 32.67 provides for a temporary seasonal endorsement for the holder of an SRL certificate to operate outside restricted limits or the inshore fishing limit.

The privilege given by the endorsement is to operate within a uniform distance of 100NM from the coast on a ship of less than 24m. The requirements to obtain this endorsement are set out in Part 32.67 and in the guidance resources on MNZ's website: maritimenz.govt.nz/seafarers

Certificates for coastal and offshore limits – non-fishing

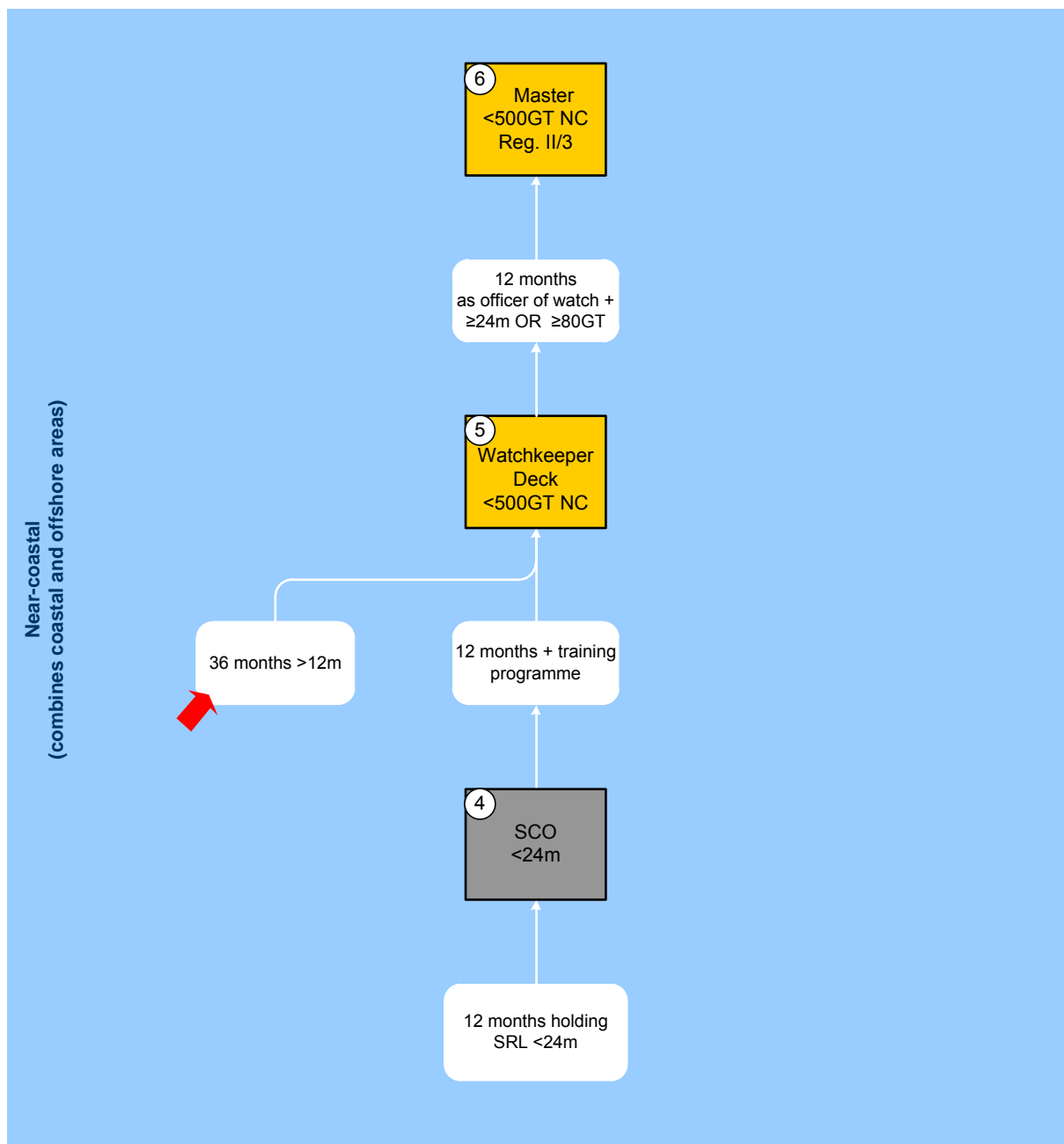


Figure 6 Coastal and offshore limits certificate – non-fishing

4 Skipper Coastal/Offshore

Short name	SCO
Regulation	This is not an STCW or STCW-F certificate
Replaces	For vessels less than 24m in length, this certificate replaces: <ul style="list-style-type: none"> NZOW with ILM endorsement (coastal) NZOM with STCW endorsement (offshore)
Operational limit	Restricted, coastal and offshore
Privileges	Skipper of passenger, non-passenger or fishing vessels less than 24m in length within coastal and offshore limits
Prior certification	SRL endorsed to 24m length overall
Minimum service	<p><i>Duration:</i> 12 months</p> <p><i>Vessel length:</i> 12m or more length overall</p> <p><i>Operational limit:</i> service may be obtained in any limit</p> <p><i>While holding:</i> SRL certificate endorsed for <24m</p>
Training and supplementary certificates	<ul style="list-style-type: none"> Completion of an accepted/approved training course First aid certificate Fire fighting Survival craft MGROC
Career progression	<p>To Watchkeeper Deck <500GT Near-Coastal, with 12 months' sea service aboard non-fishing vessels of 12m or more in length, plus STCW basic training and an accepted/approved training course and examination</p> <p>To Mate Fishing Vessel – Limited, with 12 months' sea service aboard fishing vessels of 12m or more in length, STCW-F basic safety training, and an accepted/approved training course and examination</p>

5 Watchkeeper Deck on ships less than 500 gross tonnes in near-coastal area

Short name	Watchkeeper Deck <500GT NC
Regulation	Watchkeeper Deck <500GT is an STCW certificate under Regulation II/3.3
Replaces	NZOW
Operational limit	1. Coastal and offshore (near-coastal waters)
Privileges	<ul style="list-style-type: none"> • Watchkeeper deck on commercial ships less than 500GT in coastal and offshore limits
Prior certification	<ul style="list-style-type: none"> • None or SCO
Minimum age	2. 18
Minimum service	<p><i>Duration:</i> 36 months</p> <p><i>Vessel length:</i> 12m or more overall length</p> <p><i>Conditions:</i> 18 months outside restricted limits in a deck capacity on a non-fishing vessel</p> <p>Or</p> <p><i>Duration:</i> 12 months</p> <p><i>Vessel length:</i> 12m or more overall length</p> <p><i>Training:</i> structured training programme that includes the above sea service</p> <p><i>Conditions:</i> six months of sea service must be outside restricted limits in a deck capacity on a non-fishing vessel while holding SCO</p>
Training and supplementary certificates	<ul style="list-style-type: none"> • Completion of an accepted/approved training course • STCW basic training • Proficiency in survival craft and rescue boats (other than fast rescue boats) • Advanced fire fighting • Medical first aid • MGROC
Career progression	To Master <500GT Near-Coastal, with 12 months' sea service as officer of the watch aboard non-fishing vessels ≥24m length, or ≥80GT beyond restricted limits, plus STCW advanced training

6 Master on ships less than 500 gross tonnes in near-coastal area

Short name	Master <500GT NC
Regulation	STCW Regulation II/3.5
Replaces	NZOM with STCW-95 endorsement
Operational limit	Coastal and offshore (near-coastal)
Privileges	Master of a commercial non-fishing ship less than 500GT in coastal and offshore limits
Prior certification	Watchkeeper Deck <500GT NC
Minimum service	<p><i>Duration:</i> 12 months</p> <p><i>Vessel type:</i> six months' service on non-fishing vessels</p> <p><i>Vessel length:</i> 24m or more length, or 80GT or more</p> <p><i>Operational limit:</i> outside restricted limits</p> <p><i>Role:</i> Watchkeeper deck</p>
Training and supplementary certificates	<ul style="list-style-type: none"> • Completion of an accepted/approved training course • STCW basic training • Advanced fire fighting • Proficiency in survival craft and rescue boats (other than fast rescue boats) • Medical first aid • MGROC
Career progression	<ul style="list-style-type: none"> • To Watchkeeper Deck, with 12 months' sea service on commercial ships as a person in charge of a navigational watch (for not less than eight hours of every 24 hours claimed) on vessels ≥ 24 m or ≥ 80GT that operate beyond restricted limits, while holding Master <500GT NC. Must include six months' bridge watchkeeping under the supervision of a master or deck officer, plus completion of an accepted/approved training course, STCW ancillaries and final examination

Deck certificates for fishing vessels – limited and unlimited waters

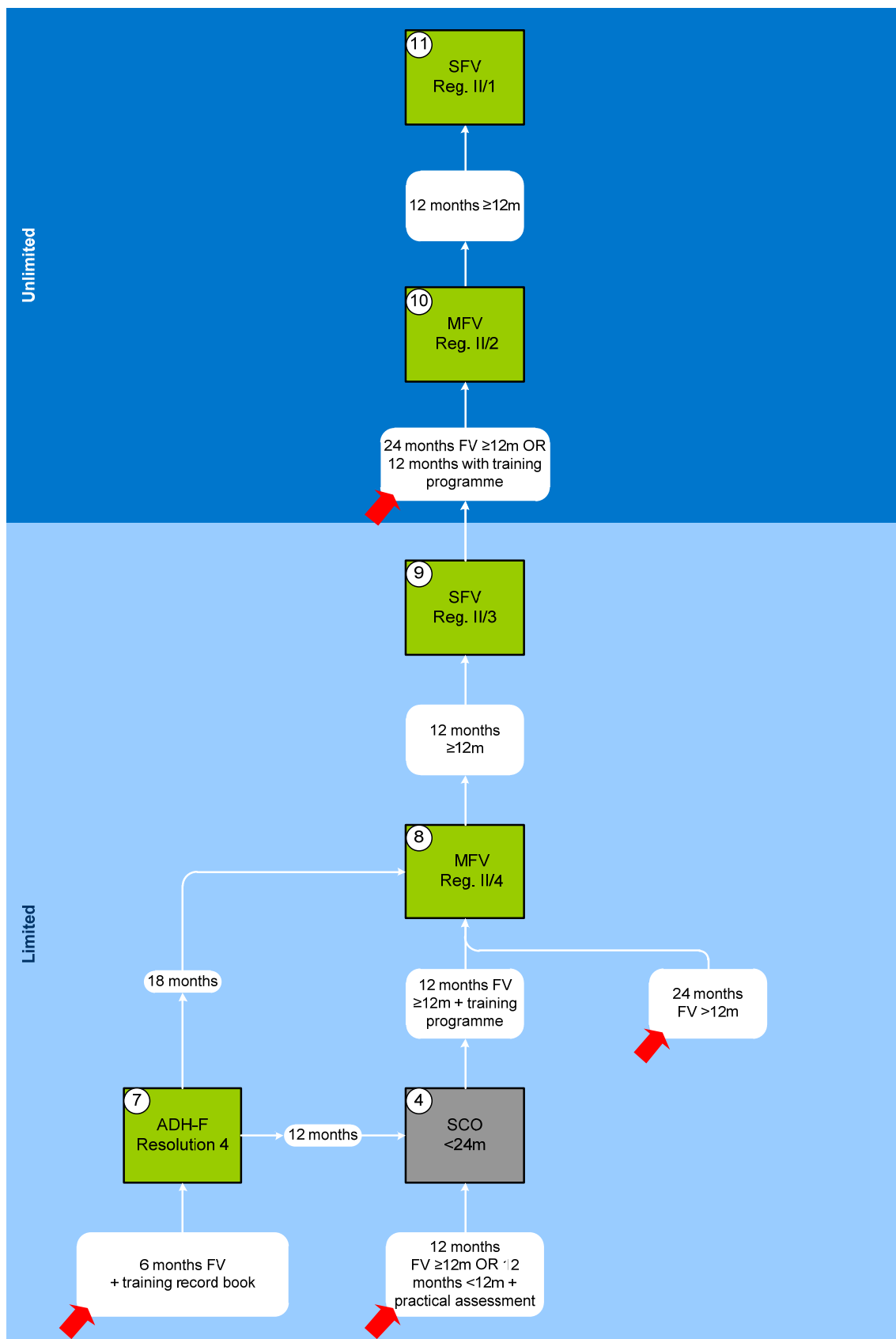


Figure 7 Fishing vessel deck certificates

7 Advanced Deckhand – Fishing

This certificate of proficiency for fishing deckhands aboard fishing vessels operating in limited and unlimited waters corresponds to the recommendations under Resolution 4 of STCW-F.

Short name	ADH-F
Regulation	STCW-F Resolution 4
Replaces	Advanced deckhand fishing endorsement
Operational limit	Coastal, offshore and unlimited
Privileges	Deckhand aboard fishing vessels of 24m or more length in limited waters and aboard unlimited fishing vessels
Prior certification	None
Minimum age	16
Minimum service	<i>Duration:</i> six months <i>Vessel type:</i> fishing <i>Operational limit:</i> outside enclosed limits
Training and supplementary certificates	<ul style="list-style-type: none"> • Training record book • Completion of an accepted/approved training course • STCW-F basic safety training
Career progression	<ul style="list-style-type: none"> • To MFV – Limited, with 18 months' sea service aboard fishing vessels $\geq 12\text{m}$, of which 12 months must be beyond restricted limits, plus completion of an accepted/approved training course and examination • To SCO, with 12 months' sea service aboard vessels $\geq 12\text{m}$ in any limit, plus completion of an accepted/approved training course and examination

8 Mate Fishing Vessel – Limited

This certificate corresponds to the certificate under Regulation II/4 of STCW-F as officer in charge of a navigational watch on fishing vessels 24–45m in length operating in limited waters.

Short Name	MFV
Regulation	STCW-F Regulation II/4
Operational limit	Coastal and offshore (limited waters)
Privileges	Mate on fishing vessels of 24–45m length
Prior certification	None, ADH-F or SCO
Minimum age	18
Minimum service	<p> <i>Duration:</i> 24 months <i>Vessel type:</i> fishing <i>Vessel length:</i> 12m or more length overall <i>Operational limit:</i> 12 months must be outside restricted limits <i>Role:</i> in a deck capacity Or <i>Duration:</i> 18 months <i>Vessel type:</i> fishing <i>Vessel length:</i> 12m or more length overall <i>Operational limit:</i> 12 months outside restricted limits <i>While holding:</i> ADH-F Or <i>Duration:</i> 12 months <i>Vessel type:</i> fishing <i>Vessel length:</i> 12m or more length overall <i>Conditions:</i> six months' sea service outside restricted limits in a deck capacity <i>While holding:</i> SCO </p>
Training and supplementary certificates	<ul style="list-style-type: none"> • Completion of an accepted/approved training course • STCW-F basic safety training • MGROC
Career progression	<p>To SFV, with 12 months' sea service as officer of the watch aboard fishing vessels ≥12m length overall outside restricted limits</p> <p>To MFV-U, with 24 months' sea service in a deck capacity aboard fishing vessels ≥12 m length overall beyond restricted limits</p>

9 Skipper Fishing Vessel – Limited

This certificate corresponds to the certificate under Regulation II/3 of STCW-F for skipper of a fishing vessel of 24–45m length operating in limited waters.

Short name	SFV
Regulation	STCW-F Regulation II/3
Replaces	NZOM
Operational limit	Coastal and offshore limits (limited waters)
Privileges	Skipper fishing vessel of 24–45m length
Prior certification	MFV
Minimum service	<p><i>Duration:</i> 12 months</p> <p><i>Vessel type:</i> fishing</p> <p><i>Vessel length:</i> 12m or more length overall</p> <p><i>Operational limit:</i> outside restricted limits</p> <p><i>Role:</i> officer of the watch</p>
Training and supplementary certificates	<ul style="list-style-type: none"> • Completion of an accepted/approved training course • STCW-F basic safety training • MGROC
Career progression	To Master <500GT NC, with 12 months' sea service as officer of the watch on vessels of ≥24 m or ≥80GT, of which six months must be on non-fishing vessels operating beyond restricted limits, while holding SFV, and meeting the standard of competence outlined in STCW Regulation II/3

10 Mate Fishing Vessel – Unlimited

This certificate corresponds to the certificate under Regulation II/2 of STCW-F as officer in charge of a navigational watch on fishing vessels of 24m or more in length operating in unlimited waters.

Short name	MFV-U
Regulation	STCW-F Regulation II/2
Replaces	The syllabus for this certificate is aligned with mate deep-sea fishing vessel
Operational limit	Any operating area
Privileges	Mate fishing vessel of 24m or more in length
Prior certification	None
Minimum age	18
Minimum service	<p><i>Duration:</i> 24 months</p> <p><i>Vessel type:</i> fishing</p> <p><i>Vessel length:</i> 12m or more length overall</p> <p><i>Operational limit:</i> outside restricted limits</p> <p><i>Role:</i> operating in a deck capacity</p>
Training and supplementary certificates	<ul style="list-style-type: none"> • Completion of an accepted/approved training course • STCW-F basic safety training • Proficiency in survival craft and rescue boats (other than fast rescue boats) • Advanced fire fighting • GMDSS Radio Operator
Career progression	<ul style="list-style-type: none"> • To SFV-U, with 12 months' sea service aboard fishing vessels of ≥12m operating beyond restricted limits as mate or skipper, plus completion of an accepted/approved training course and examination

11 Skipper Fishing Vessel – Unlimited

This certificate corresponds to the certificate under Regulation II/1 of STCW-F for skipper on fishing vessels of 24m or more in length operating in unlimited waters.

Short name	SFV-U
Regulation	STCW-F Regulation II/1
Replaces	Master deep-sea fishing vessel
Operational limit	Any operating area
Privileges	Master on fishing vessels of 24m or more in length
Prior certification	Mate fishing vessel – unlimited
Minimum service	<p><i>Duration:</i> 12 months</p> <p><i>Vessel type:</i> fishing</p> <p><i>Vessel length:</i> 12m or more length overall</p> <p><i>Operational limit:</i> outside restricted limits</p> <p><i>While holding:</i> MFV-U certificate</p> <p><i>Role:</i> operating as mate or skipper</p> <p><i>Conditions:</i> up to six months may be substituted from sea service as officer in charge of a navigational watch on vessels covered by STCW</p>
Training and supplementary certificates	<ul style="list-style-type: none"> • Completion of an accepted/approved training course • STCW-F basic safety training • Advanced fire fighting • Proficiency in survival craft and rescue boats (other than fast rescue boats) • Medical first aid • GMDSS Radio Operator
Career progression	This is the most senior certificate for the command of fishing vessels

Deck certificates for the unlimited area

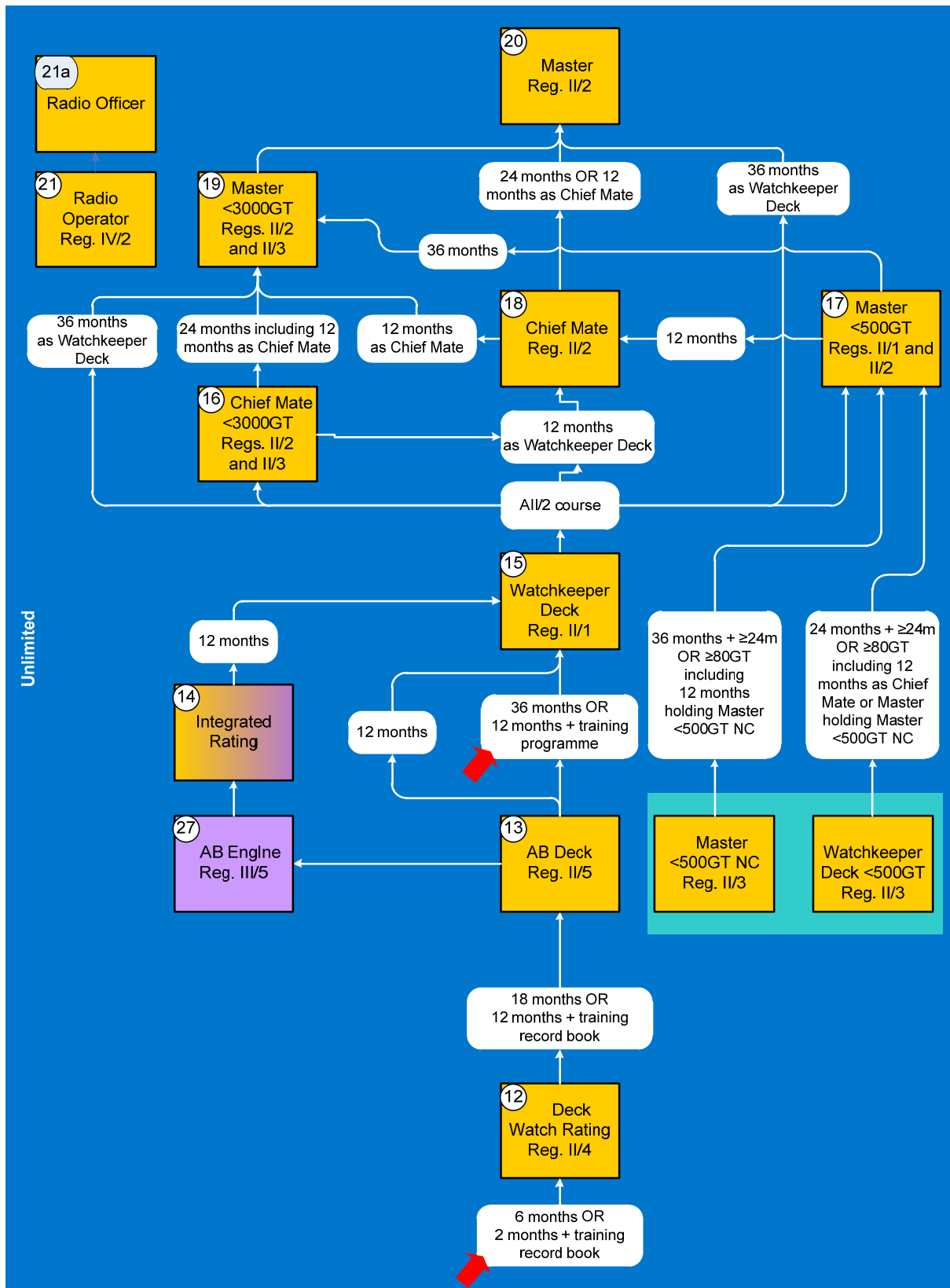


Figure 8 Unlimited area deck certificates

12 Deck Watch Rating

This is an STCW certificate of proficiency for ratings forming part of a navigational watch.

Short name	DWR
Regulation	STCW Regulation II/4
Replaces	Competencies and seagoing experience for this certificate closely align with the previous DWR certificate
Operational limit	Any operating limit
Privileges	<ul style="list-style-type: none"> Rating forming part of a navigational watch Ships of 500GT or more
Prior certification	None
Minimum age	16
Minimum service	<p><i>Duration:</i> six months' training and sea service</p> <p><i>Vessel size:</i> 500GT or more</p> <p>Or</p> <p><i>Duration:</i> two months</p> <p><i>Vessel size:</i> 500GT or more</p> <p><i>Training:</i> either pre-sea or aboard ship, documented in a training record book</p> <p>(The ISF On Board Training Record Book based on the competence requirements of the 2010 amendments to the STCW Convention is acceptable as on-board training)</p>
Training and supplementary certificates	<ul style="list-style-type: none"> Completion of an accepted/approved training course (for six months option) Completion of an approved training record book (for two months option) STCW basic training Steering certificate
Career progression	To AB Deck, with 18 months' sea service on ships of 500GT or more or with 12 months' sea service on ships of 500GT or more and an approved training programme

13 Able Seafarer Deck

Short name	AB Deck
Regulation	Certificate of proficiency under STCW Regulation II/5
Replaces	This certificate closely aligns to the previous ILO able seaman certificate
Operational limit	Any operating limit
Privileges	<ul style="list-style-type: none"> • AB deck • Ships of any gross tonnage
Prior certification	DWR
Minimum age	18
Minimum service	<p><i>Duration:</i> 18 months</p> <p><i>Vessel size:</i> 500GT or more</p> <p><i>While holding</i> DWR certificate</p> <p>Or</p> <p><i>Duration:</i> 12 months</p> <p><i>Vessel size:</i> 500GT or more</p> <p><i>While holding:</i> DWR certificate</p> <p><i>Training:</i> approved training (ISF On Board Training Record Book based on the competence requirements of the STCW Convention 2010 amendments is acceptable as approved training)</p>
Training and supplementary certificates	<p>If you choose not to complete structured on-board training that is recorded in an approved training record book, you must attend and pass an AB Deck training course that is acceptable to the MNZ Director and meets the competencies set out in section A-II/5 of the STCW code.</p> <ul style="list-style-type: none"> • Completion of an accepted/approved training course • STCW basic training • Proficiency in survival craft and rescue boats (other than fast rescue boats)
Career progression	<p>Certificate of proficiency as Integrated Rating, if holding current certificates of proficiency as AB Deck and AB Engine</p> <p>To Watchkeeper Deck, with 12 months' sea service on ships > 500GT, including six months' bridge watchkeeping under the supervision of the master or a deck officer, plus an accepted/approved training course and examination</p>

14 Integrated Rating

Short name	IR
Regulation	Certificate of proficiency under STCW Regulations II/5 and III/5
Replaces	New certificate
Operational limit	Any operating limit
Privileges	<ul style="list-style-type: none"> • Integrated rating, where required by a minimum safe crewing document • Ships of any gross tonnage and propulsion power
Prior certification	AB Deck and AB Engine
Minimum service	As required for AB Deck and AB Engine
Training and supplementary certificates	<ul style="list-style-type: none"> • STCW basic training • Proficiency in survival craft and rescue boats (other than fast rescue boats)
Career progression	<p>To Watchkeeper Deck, with 12 months' sea service on ships of 500GT or more while holding AB Deck or an IR certificate of proficiency, including six months' bridge watchkeeping under the supervision of the master or a deck officer, plus completion of an accepted/approved training course and examination</p> <p>To MEC 3, with 18 months' combined workshop skills training and approved sea service in the engine department of a ship of 750kW or more while holding AB Engine or an IR certificate of competency, including six months of engine room watchkeeping duties under supervision of the chief engineer officer or an engineer officer aboard ships with 750kW main propulsion power or more</p>

15 Watchkeeper Deck

Short name	Watchkeeper Deck
Regulation	STCW Regulation II/1
Replaces	Officer in charge of a navigational watch of a foreign-going ship
Operational limit	Any operating limit
Privileges	Officer in charge of a navigational watch on ships of any gross tonnage
Prior certification	None
Minimum age	18
Minimum service	<p><i>Duration:</i> 12 months' sea service on commercial ships outside restricted limits on voyages of at least 24 hours</p> <p><i>Vessel size:</i> ≥500GT</p> <p><i>Training:</i> approved training programme, including on-board training documented in approved training record book</p> <p><i>Conditions:</i> include six months' bridge watchkeeping under supervision of master or deck officer</p> <p>Or</p> <p><i>Duration:</i> 36 months</p> <p><i>Vessel size:</i> at least 12 months' sea service on commercial ships ≥500GT and remaining 24 months' sea service on commercial ships of ≥24m or ≥80GT outside restricted limits, of which at least 12 months must be on voyages of at least 24 hours</p> <p><i>Conditions:</i> include six months' bridge watchkeeping under supervision of master or deck officer</p>
Training and supplementary certificates	<ul style="list-style-type: none"> • Completion of an accepted/approved training course • Approved training record book as part of sea service • STCW basic training • Advanced fire fighting • Proficiency in survival craft and rescue boats (other than fast rescue boats) • Medical first aid • GMDSS Radio Operator
Career progression	<p>To Chief Mate, with 12 months' sea service as Watchkeeper Deck, plus completion of an accepted/approved training course and examination</p> <p>To Master, with 36 months' sea service as Watchkeeper Deck, plus •completion of an accepted/approved training course and examination</p> <p>To Chief Mate <3000GT, with no additional sea service but STCW section A-II/2.5 competencies attained</p> <p>To Master <500GT Unlimited, with STCW section A-II/2 competencies attained for a vessel limited by its tonnage</p>

16 Chief Mate on ships less than 3000 gross tonnes

Short name	Chief Mate <3000GT
Regulation	STCW Regulation II/2.3
Replaces	New certificate
Operational limit	Any operating limit
Privileges	<ul style="list-style-type: none"> • Chief mate • Ships of less than 3000 gross tonnage • Watchkeeper deck on ships of any gross tonnage in any operating limit
Prior certification	Watchkeeper Deck
Minimum service	<p><i>Duration:</i> no additional sea service required</p> <p><i>While holding:</i> Watchkeeper Deck</p>
Training and supplementary certificates	<ul style="list-style-type: none"> • Completion of a training course taking into account section A-II/2.5 of STCW code • STCW basic training • Advanced fire fighting • Proficiency in survival craft and rescue boats (other than fast rescue boats) • Medical care on board ship • GMDSS Radio Operator
Career progression	To Master <3000GT, with 12 months' sea service as Chief Mate and additional 12 months' sea service as Watchkeeper Deck

17 Master on ships less than 500 gross tonnes

Short Name	Master <500GT
Regulation	STCW Regulation II/2
Replaces	NZOM with STCW endorsement and Unit Standards 6912 and 6913
Operational limit	Any operating limit
Privileges	<ul style="list-style-type: none"> • Master or chief mate • Ships of less than 500 gross tonnes • Watchkeeper deck on ships of any gross tonnage in any operating area
Prior certification	Master <500GT Near-Coastal or Watchkeeper Deck <500GT Near-Coastal
Minimum service	<p><i>Duration:</i> 36 months' sea service as officer in charge of a navigational watch on ships of 24m or more or 80GT or more</p> <p><i>While holding:</i> Master <500GT Near-Coastal or Watchkeeper Deck<500GT Near-Coastal</p> <p><i>Conditions:</i> at least 12 months' sea service served while holding Master <500GT Near-Coastal and at least 24 months served on ships on voyages of at least 24 hours' duration</p> <p>Or</p> <p><i>Duration:</i> 24 months' sea service as officer in charge of a navigational watch aboard ships of 24m or more or 80GT or more, at least 12 months of which is served as chief mate or master</p> <p><i>While holding:</i> Master <500GT Near-Coastal</p>
Training and supplementary certificates	<ul style="list-style-type: none"> • Completion of an accepted/approved training course that meets the competencies specified in section A-II/1 and relevant sections of A-II/2 of the STCW code for a vessel limited by its tonnage (approved modules in command navigation, shipmaster's business and management, and ship operations and administration) • STCW basic training • Advanced fire fighting • Medical care on board ship • Proficiency in survival craft and rescue boats (other than fast rescue boats) • GMDSS Radio Operator
Career progression	<p>To Chief Mate, with 12 months' sea service</p> <p>If six months' qualifying sea service is served on commercial ships ≥500GT that are not fishing vessels, the holder can gain an endorsement as Chief Mate <3000GT Near-Coastal.</p> <p>If 24 months of qualifying sea service is served on commercial ships ≥500GT that are not fishing vessels, the certificate holder can gain an endorsement as Master <3000GT Near-Coastal.</p>

18 Chief Mate

Short name	Chief Mate
Regulation	STCW Regulation II/2
Replaces	Chief mate of a foreign-going ship
Operational limit	Any operating limit
Privileges	<ul style="list-style-type: none"> • Chief mate • Ships of any gross tonnage
Prior certification	Watchkeeper Deck
Minimum service	<p><i>Duration:</i> 12 months</p> <p><i>Vessel size:</i> 500GT or more</p> <p><i>While holding:</i> Watchkeeper Deck</p>
Training and supplementary certificates	<ul style="list-style-type: none"> • Completion of an accepted/approved training course • STCW basic training • Advanced fire fighting • Medical care on board ship • Proficiency in survival craft and rescue boats (other than fast rescue boats) • GMDSS Radio Operator
Career progression	To Master, with 24 months' sea service or 12 months' sea service as chief mate.

19 Master on ships less than 3000 gross tonnes

Short name	Master <3000GT
Regulation	STCW Regulation II/2.3
Replaces	Master of a foreign-going ship of less than 3000 tonnes gross tonnage
Operational limit	Any operating limit
Privileges	<ul style="list-style-type: none"> • Master or chief mate • Ships of less than 3000 gross tonnage
Prior certification	Watchkeeper Deck
Minimum service	<p><i>Duration:</i> 36 months</p> <p><i>Vessel size:</i> ≥500GT</p> <p><i>While holding:</i> Watchkeeper Deck</p> <p><i>Role:</i> Officer in charge of a navigational watch</p> <p>Or</p> <p><i>Duration:</i> 24 months</p> <p><i>Vessel size:</i> ≥500GT</p> <p><i>While holding:</i> Watchkeeper Deck and either Chief Mate <3000GT or Chief Mate</p> <p><i>Role:</i> Operating for 12 months as chief mate</p>
Training and supplementary certificates	<ul style="list-style-type: none"> • Completion of an accepted/approved training course • STCW basic training • Advanced fire fighting • Medical care on board ship • Proficiency in survival craft and rescue boats (other than fast rescue boats) • GMDSS Radio Operator
Career progression	To Master, with approved education and training and demonstrated competence as specified in section A-II/2 for master of a ship of 3000GT or more

20 Master

Short name	Master
Regulation	STCW Regulation II/2
Replaces	Master of a foreign-going ship (master mariner)
Operational limit	Any operating limit
Privileges	<ul style="list-style-type: none"> • Master • Ships of any gross tonnage
Prior certification	Watchkeeper Deck, Chief Mate, Master <3000GT
Minimum service	<p><i>Duration:</i> 36 months</p> <p><i>Vessel size:</i> ≥500 GT</p> <p><i>While holding:</i> Watchkeeper Deck</p> <p><i>Role:</i> Officer in charge of a navigational watch</p> <p>Or</p> <p><i>Duration:</i> 24 months</p> <p><i>Vessel size:</i> ≥500GT</p> <p><i>While holding:</i> Watchkeeper Deck and either Chief Mate <3000GT or Chief Mate</p> <p><i>Role:</i> Operating for 12 months' as chief mate</p>
Training and supplementary certificates	<ul style="list-style-type: none"> • Completion of an accepted/approved training course • STCW basic training • Advanced fire fighting • Medical care on board ship • Proficiency in survival craft and rescue boats (other than fast rescue boats) • GMDSS Radio Operator
Career progression	This is the most senior command certificate

21 GMDSS Radio Operator

Short name	GMDSS Radio Operator
Regulation	STCW Regulation IV/2
Replaces	Existing certificate
Operational limit	Any operating limit
Privileges	Primarily to perform, as a navigational watchkeeper at operational level, the functions and duties of a radio operator on a ship required to participate in the GMDSS to which Subpart B of Part 31 applies
Prior certification	None
Minimum service	Not applicable
Training and supplementary certificates	<ul style="list-style-type: none"> • Meet competencies in accordance with section A-IV/2 of the STCW code • MGOC
Career progression	Not applicable

21a Radio Officer

Short name	Radio Officer
Regulation	STCW Regulation IV/2
Replaces	Existing certificate
Operational limit	Any operating limit
Privileges	Perform, at operational level, the functions and duties of a radio officer on a ship required to participate in the GMDSS, where the person's shipboard duties are only to operate the radio equipment
Prior certification	None
Minimum service	Not applicable
Training and supplementary certificates	<ul style="list-style-type: none"> • Demonstrates competencies in accordance with section A-IV/2 of the STCW code • STCW basic training • MGOC
Career progression	Not applicable

Certificates for sailing vessels and superyachts

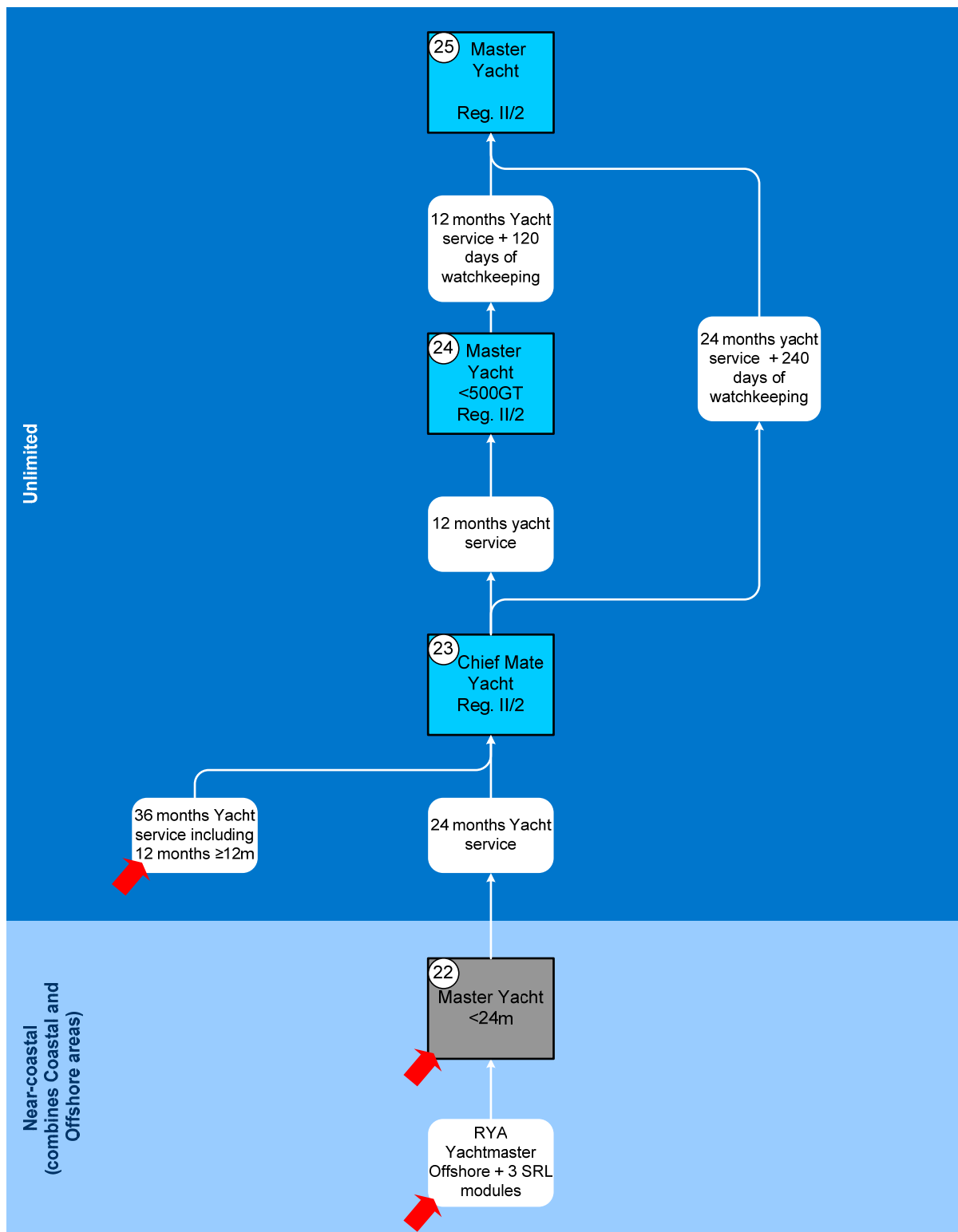


Figure 9 Sailing vessel and superyacht certificates

22 Master Yacht less than 24 metres

Short name	Master Yacht <24m
Regulation	This is a non-STCW certificate
Replaces	New certificate
Operational limit	Restricted, coastal and offshore (near-coastal)
Privileges	Master of commercial sailing ships of less than 24m length.
Prior certification	None
Minimum age	18
Minimum service	<p><i>Duration:</i> 50 days (2500NM) on sailing ships only (if on pleasure craft, must be as skipper)</p> <p><i>Conditions:</i> includes at least five passages over 60NM, two overnight and two as skipper</p>
Training and supplementary certificates	<ul style="list-style-type: none"> • Satisfactory completion of RYA Yachtmaster Offshore course • Completion of tasks in approved training record book for Skipper Restricted Limits for electronic navigation, management of vessel operations, management of vessel safety and compliance³ • Practical assessment through RYA Yachtmaster Offshore course • Practical assessment of required additional MNZ competencies (if training record book completed aboard a pleasure craft) • MNZ final examination • First aid certificate • MGROC • Square-rigged endorsement necessary, where appropriate
Career progression	<p>To Chief Mate Yacht, either with 36 months' sea service operating in a deck capacity, of which at least 12 months must be aboard vessels of 12m or more in length operating beyond restricted limits, including six months' navigational watchkeeping duties under supervision of a master or deck officer</p> <p>Or with 24 months' yacht sea service including at least 500 days' actual sea service and 230 days of any combination of actual sea service, stand-by service or yard service, and including six months' navigational watchkeeping duties under supervision of a master or deck officer</p>

³ The training record book includes training and demonstrating competence in the fire fighting and survival craft ancillary proficiencies set out in the Schedule to Part 32.

23 Chief Mate Yacht

Short name	Chief Mate Yacht
Regulation	STCW Regulation II/2
Replaces	Chief mate (yacht)
Operational limit	Any operating area
Privileges	Chief mate of a superyacht less than 3000GT
Prior certification	None
Minimum age	18
Minimum service	<p><i>Duration:</i> 36 months documented in an approved training record book</p> <p><i>Role:</i> operating in a deck capacity and including six months' navigational watchkeeping duties under the supervision of a master or deck officer</p> <p><i>While holding:</i> SRL</p> <p><i>Conditions:</i> 12 months served on vessels of 12m or more in length outside restricted limits</p> <p>Or</p> <p><i>Duration:</i> 24 months' yacht sea service, including at least 500 days' actual sea service and 230 days of any combination of actual sea service, stand-by service or yard service, and including six months' navigational watchkeeping duties under supervision of a master or deck officer</p>
Training and supplementary certificates	<ul style="list-style-type: none"> • Completion of approved training record book • Completion of an accepted/approved training course • STCW basic training • Proficiency in survival craft and rescue boats (other than fast rescue boats) • Advanced fire fighting • Medical care on board ship • GMDSS Radio Operator
Career progression	<p>To Master Yacht <500GT, with 12 months' on-board yacht service, including 120 days in charge of a navigational watch while holding Chief Mate Yacht</p> <p>To Master Yacht, with 24 months' on-board yacht service, including 12 months on a ship of ≥24m or ≥80GT, or six months on a ship of ≥500GT, and including 240 days in charge of a navigational watch while holding Chief Mate Yacht</p>

24 Master Yacht less than 500 gross tonnes

Short name	Master Yacht <500GT
Regulation	STCW Regulation II/2
Replaces	New Zealand master (yacht less than 500GT)
Operational limit	Any operating area
Privileges	Master of a superyacht less than 500GT
Prior certification	Chief Mate Yacht
Minimum service	<p><i>Duration:</i> 12 months on-board yacht service</p> <p><i>While holding:</i> Chief Mate Yacht</p> <p><i>Conditions:</i> at least 120 days in charge of navigational watch</p>
Training and supplementary certificates	<ul style="list-style-type: none"> • Completion of an accepted/approved training course • STCW basic training • Proficiency in survival craft and rescue boats (other than fast rescue boats) • Advanced fire fighting • Medical care on board ship • GMDSS Radio Operator
Career progression	To Master Yacht, with 12 months' on-board yacht service, including six months on a ship of ≥24m or ≥80GT or three months on a ship of ≥500GT, and 120 days in charge of a navigational watch while holding Master Yacht <500GT

25 Master Yacht

Short name	Master Yacht
Regulation	STCW Regulation II/2
Replaces	New Zealand master (yacht)
Operational limit	Any operating limit
Privileges	Master of a superyacht less than 3000GT
Prior certification	Chief Mate Yacht or Master Yacht <500GT
Minimum service	<p>12 months' on-board yacht service, including six months on a ship of $\geq 24\text{m}$ or $\geq 80\text{GT}$ or three months on a ship of $\geq 500\text{GT}$, and 120 days in charge of a navigational watch while holding Master Yacht <500GT</p> <p>Or</p> <p>24 months' on-board yacht service, including 12 months on a ship of $\geq 24\text{m}$ or $\geq 80\text{GT}$ or six months on a ship of $\geq 500\text{GT}$, and 240 days in charge of a navigational watch while holding Chief Mate Yacht</p>
Training and supplementary certificates	<ul style="list-style-type: none"> • Completion of an accepted/approved training course • STCW basic training • Proficiency in survival craft and rescue boats (other than fast rescue boats) • Advanced fire fighting • Medical care on board ship • GMDSS Radio Operator
Career progression	This is the most senior superyacht command certificate of competency

Engineering certificates

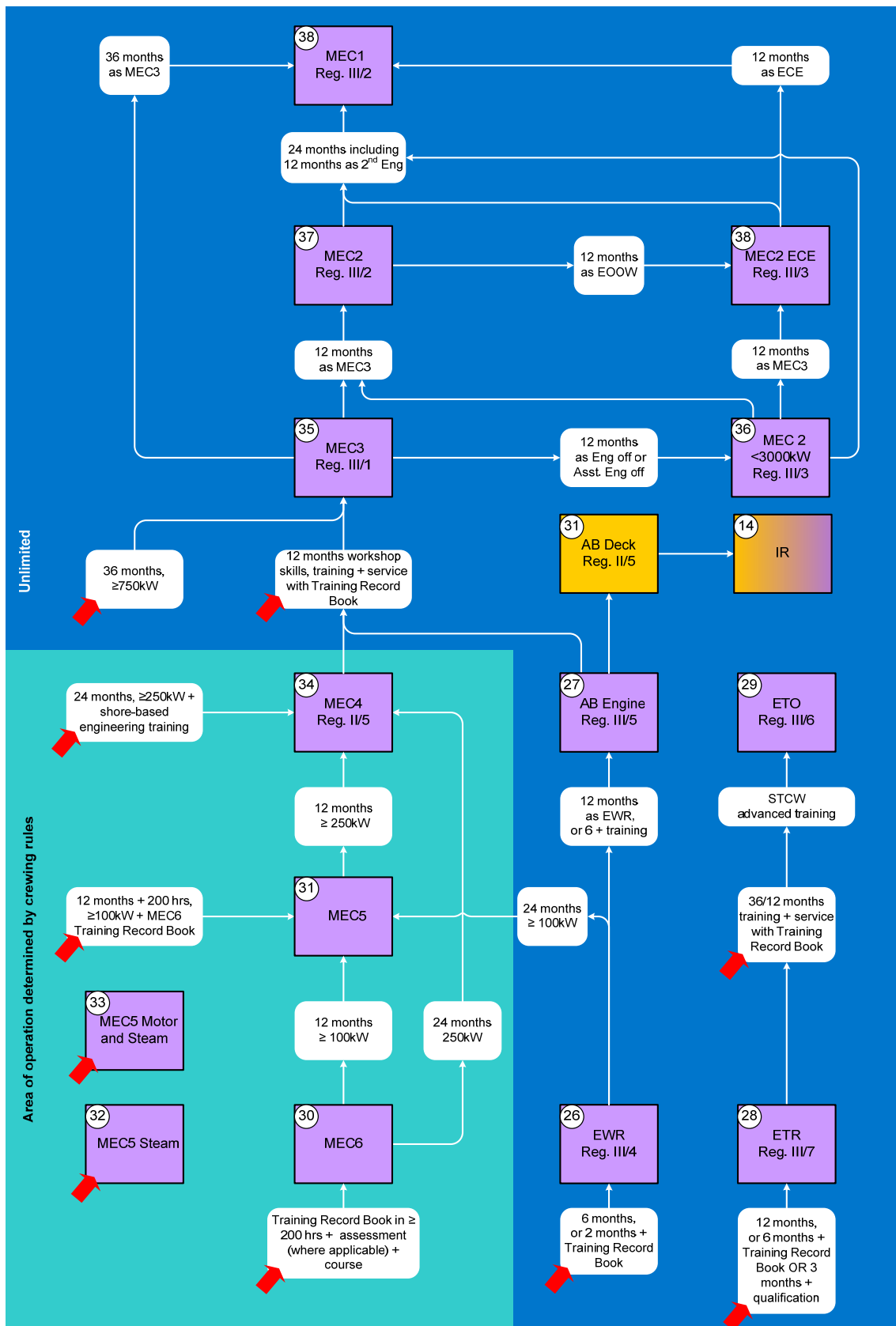


Figure 10 Engineering certificates

26 Engine Room Watch Rating

Short name	EWR
Regulation	Certificate of proficiency under STCW Regulation III/4
Replaces	Engine room watch rating
Operational limit	Any operating limit
Privileges	<ul style="list-style-type: none"> Rating forming part of a watch in a manned engine room or designated to perform duties in a periodically unmanned engine room Ships powered by main propulsion machinery of 750kW propulsion power or more
Prior certification	None
Minimum age	16
Minimum service	<p><i>Duration:</i> six months' training and experience</p> <p><i>Engine power:</i> 750kW main propulsion power or more</p> <p>Or</p> <p><i>Duration:</i> two months</p> <p><i>Engine power:</i> 750kW main propulsion power or more</p> <p><i>Training:</i> either on board ship, documented in a training record book, or shore-based course (The ISF On Board Training Record Book based on the competence requirements of the 2010 amendments to the STCW Convention is acceptable as on-board training)</p>
Training and supplementary certificates	<ul style="list-style-type: none"> Completion of an accepted/approved training course (for six months option) Completion of an approved training record book (for two months option) STCW basic training
Career progression	<p>To AB Engine, with either 12 months' sea service aboard ships with 750kW main propulsion power or more, or six months' sea service aboard ships with 750kW main propulsion power or more plus completion of an approved training programme</p> <p>To MEC 5, with 24 months' sea service while holding EWR and performing duties on main machinery propulsion for not less than eight of every 24 hours claimed</p>

27 Able Seafarer Engine

Short name	AB Engine
Regulation	Certificate of proficiency under STCW Regulation III/5
Replaces	New certificate
Operational limit	Any operating limit
Privileges	<ul style="list-style-type: none"> AB engine in a manned engine room or designated to perform duties in a periodically unmanned engine room Ship powered by main propulsion machinery of 750kW propulsion power or more
Prior certification	EWR
Minimum age	18
Minimum service	<p><i>Duration:</i> 12 months</p> <p><i>Engine power:</i> 750kW main propulsion power or more</p> <p><i>While holding:</i> EWR</p> <p>Or</p> <p><i>Duration:</i> six months</p> <p><i>Engine power:</i> 750kW main propulsion power or more</p> <p><i>While holding:</i> EWR</p> <p><i>Training:</i> approved training (The ISF On Board Training Record Book based on the competence requirements of the 2010 amendments to the STCW Convention is acceptable as approved training)</p>
Training and supplementary certificates	<ul style="list-style-type: none"> Completion of an accepted/approved training course STCW basic training
Career progression	<p>A certificate of proficiency as Integrated Rating requires current certificates for AB Engine and AB Deck</p> <p>To MEC 3, with 18 months' combined workshop skills training and approved sea service in the engine department while holding AB Engine, including six months' engine room watchkeeping duties under supervision of the chief engineer officer or an engineer officer, aboard ships with 750kW main propulsion power or more</p>

28 Electro-technical Rating

Short name	ETR
Regulation	Certificate of proficiency under STCW Regulation III/7
Replaces	New certificate
Operational limit	Any operating limit
Privileges	Electro-technical rating on a seagoing ship powered by main propulsion machinery of 750kW propulsion power or more
Prior certification	None
Minimum age	18
Minimum service	<p><i>Duration:</i> 12 months' training and experience</p> <p><i>Engine power:</i> 750kW main propulsion power or more</p> <p>Or</p> <p><i>Duration:</i> six months</p> <p><i>Engine power:</i> 750kW main propulsion power or more</p> <p><i>Training:</i> approved training</p> <p>Or</p> <p><i>Duration:</i> three months</p> <p><i>Engine power:</i> 750kW main propulsion power or more</p> <p><i>Training:</i> certificates that meet technical competencies in STCW Table III/7</p>
Training and supplementary certificates	<ul style="list-style-type: none"> • Complete approved training as above • STCW basic training
Career progression	<p>To Electro-technical Officer:</p> <p>either with six months' sea service as part of an approved programme of on-board training, documented in a training record book, along with an approved training programme that includes at least six months' workshop skills training (additional to the above sea service), aboard ships with 750kW main propulsion power or more</p> <p>or with 30 months' sea service working in the engine department aboard ships with 750kW main propulsion power or more, including workshop skills training of six months (additional to the sea service)</p>

29 Electro-technical Officer

Short name	ETO
Regulation	STCW Regulation III/6
Replaces	New certificate
Operational limit	Any operating limit
Privileges	Electro-technical officer on a seagoing ship powered by main propulsion machinery of 750kW propulsion power or more
Prior certification	None
Minimum age	18
Minimum service	<p><i>Duration:</i> 12 months' combined sea service and workshop skills training, including on-board training documented in a training record book (which includes six months' sea service as part of an approved programme of on-board training)</p> <p><i>Engine power:</i> 750kW main propulsion power or more</p> <p>Or</p> <p><i>Duration:</i> 36 months' combined workshop skills training and sea service, of which 30 months must be sea service in the engine department</p> <p><i>Role:</i> working in the engine department</p> <p><i>Engine power:</i> 750kW main propulsion power or more</p>
Training and supplementary certificates	<ul style="list-style-type: none"> • Completion of an accepted/approved training course • STCW basic training • Advanced fire fighting • Proficiency in survival craft and rescue boats (other than fast rescue boats) • Medical first aid
Career progression	This is the most senior electro-technical certificate

30 Marine Engineer Class 6

Short name	MEC 6
Regulation	Non-STCW and non-STCW-F certificate
Replaces	MEC 6
Operational limit	Any operating limit
Privileges	<p>To perform functions and duties on a motor ship as:</p> <ul style="list-style-type: none"> • engineer on fishing, passenger and non-passenger ships up to 3000kW propulsion power with systems or warranty conditions (or both) within enclosed water limits • engineer on passenger ships of up to 750kW propulsion power within inshore limits for ships with fewer than four systems • engineer on non-passenger ships of less than 24m of any propulsion power within coastal limits, that have engine and system maintenance carried out ashore or under warranty • second engineer on fishing ships of propulsion power up to 750kW in the unlimited area • engineer on fishing ships of less than 24m of any propulsion power within offshore and coastal limits, that have fewer than four systems
Prior certification	None
Minimum age	18
Minimum service	<p><i>Duration:</i> 200 hours</p> <p><i>Vessel type:</i> any powered vessel, including pleasure craft</p>
Training and supplementary certificates	<ul style="list-style-type: none"> • Completion of an on-board training record book • Practical assessment if sea service is aboard a pleasure craft • Completion of an accepted/approved training course • First aid certificate
Career progression	<p>To MEC 5, with 12 months' sea service aboard vessels of at least 100kW</p> <p>To MEC 4, with 24 months' sea service aboard vessels of 250kW or more operating beyond restricted limits</p>

31 Marine Engineer Class 5

Short name	MEC 5
Regulation	STCW-F certificate under Regulation II/5 (not an STCW certificate)
Replaces	MEC 5
Operational limit	Any operating limit
Privileges	<p>To perform functions and duties on a motor ship as:</p> <ul style="list-style-type: none"> • engineer on passenger ships of less than 24m of any propulsion power within coastal limits • engineer on passenger ships of up to 3000kW propulsion power within inshore limits, on ships that have fewer than four systems and engine and system maintenance carried out ashore or under warranty • engineer on non-passenger ships of less than 24m of any propulsion power within offshore limits • engineer on non-passenger ships of any propulsion power within restricted limits • second engineer on fishing ships of any propulsion power in any operating area • engineer on fishing ships of less than 3000kW propulsion power within inshore limits, on ships that have fewer than four systems and engine and system maintenance carried out ashore or under warranty
Prior certification	None
Minimum service	<p><i>Duration:</i> 12 months</p> <p><i>Engine power:</i> 100kW or more</p> <p><i>While holding:</i> MEC 6</p> <p>Or</p> <p><i>Duration:</i> 24 months</p> <p><i>Engine power:</i> 100kW or more</p> <p><i>Conditions:</i> completion of EWR training record book</p> <p>Or</p> <p><i>Duration:</i> 12 months plus 200 hours</p> <p><i>Engine power:</i> 100kW or more</p> <p><i>Conditions:</i> completion of MEC 6 on-board training record book</p>
Training and supplementary certificates	<ul style="list-style-type: none"> • Completion of an accepted/approved training course • STCW or STCW-F basic safety training
Career progression	To MEC 4, with 12 months' sea service aboard ships with main propulsion machinery of 250kW or more, operating beyond restricted limits

32 Marine Engineer Class 5 – Steam

Short name	MEC 5 Steam
Regulation	MEC 5 is not an STCW certificate
Replaces	MEC 5 steam
Operational limit	Any operating limit
Privileges	<p>To perform functions and duties as an engineer:</p> <ul style="list-style-type: none"> on passenger or non-passenger steam ships of any length and propulsion power that do not proceed beyond restricted limits on passenger and non-passenger steam ships of less than 24m in length and less than 500GT that operate beyond restricted limits but within coastal or offshore limits
Prior certification	None
Minimum service	<ul style="list-style-type: none"> either 36 months' sea service, including 12 months within the last 10 years, on commercial ships or New Zealand warships performing duties on steam engines or boilers or 36 months' operating experience, including 12 months within the last 10 years, performing duties on steam engines or boilers of a land installation or either 12 months' sea service within the last 10 years on commercial ships or New Zealand warships performing duties on steam engines or boilers or 12 months' operating experience within the last 10 years with steam engines or boilers of a land installation, plus two years' craft, industrial or other training of a practical nature that is acceptable to the MNZ Director or the total amount of sea service described in the options above is reduced by three months if the applicant completes training the MNZ Director accepts in reduction of sea service under Rule 32.175 and successfully complete an approved training record book while on board a steam ship during the five years before the date of application
Training and supplementary certificates	<ul style="list-style-type: none"> Completion of an accepted/approved training course STCW or STCW-F basic safety training
Career progression	To MEC 4, with 12 months' sea service aboard ships with non-steam main propulsion machinery of 250kW or more, beyond restricted limits

33 Marine Engineer Class 5 – Motor and Steam

Short name	MEC 5 Motor and Steam
Regulation	Not an STCW certificate
Replaces	New certificate
Operational limit	Any operating limit
Privileges	MEC 5 motor and steam have: <ul style="list-style-type: none"> the privileges of MEC 5 Motor and the privileges of MEC 5 Steam.
Prior certification	None
Minimum service	<ul style="list-style-type: none"> either 36 months' sea service on commercial ships or New Zealand warships within the last 10 years, including: <ul style="list-style-type: none"> 30 months performing duties on diesel engines and six months performing duties on steam propulsion engines or boilers or 12 months as engineer on a ship of 100kW propulsion power or more while holding a certificate as MEC 6 or equivalent, including: <ul style="list-style-type: none"> six months performing duties on diesel engines and six months performing duties on steam propulsion engines or boilers or 24 months on a ship of 100kW propulsion power or more, including: <ul style="list-style-type: none"> 18 months performing duties on diesel engines and six months performing duties on steam propulsion engines or boilers and successfully completing an approved training record book while on board a steam ship during the five years before the date of application.
Training and supplementary certificates	<ul style="list-style-type: none"> Completion of an accepted/approved training course STCW or STCW-F basic safety training
Career progression	To MEC 4, with 12 months' sea service aboard ships with main propulsion machinery of 250kW or more, beyond restricted limits

34 Marine Engineer Class 4

Short name	MEC 4
Regulation	STCW-F certificate under Regulation II/5 (not an STCW certificate)
Replaces	MEC 4
Operational limit	Any operational limit
Privileges	<p>To perform the functions and duties on a motor ship as chief engineer:</p> <ul style="list-style-type: none"> on passenger and non-passenger ships of less than 24m of any propulsion power, within coastal and offshore limits on passenger and non-passenger ships under 3000GT of any vessel length and propulsion power, within restricted limits on fishing ships of less than 3000kW, in the unlimited area
Prior certification	None
Minimum service	<p><i>Duration:</i> 12 months outside restricted limits</p> <p><i>Engine power:</i> 250kW or more</p> <p><i>While holding:</i> MEC 5 certificate</p> <p>Or</p> <p><i>Duration:</i> 12 months outside restricted limits</p> <p><i>Engine power:</i> 250kW or more</p> <p><i>Training:</i> shore-based engineering training</p> <p>Or</p> <p><i>Duration:</i> 24 months outside restricted limits</p> <p><i>Engine power:</i> 250kW or more</p> <p><i>While holding:</i> MEC 6</p>
Training and supplementary certificates	<ul style="list-style-type: none"> Completion of an accepted/approved training course STCW-F basic safety training advanced fire fighting proficiency in survival craft and rescue boats (other than fast rescue boats)
Career progression	To MEC 3, with 12 months' combined workshop skills training and approved sea service in the engine department while holding MEC 4, including six months' engine room watchkeeping duties under supervision of the chief engineer officer or an engineer officer, aboard ships with 750kW main propulsion power or more

35 Marine Engineer Class 3

Short name	MEC 3
Regulation	STCW Regulation III/1
Replaces	MEC 3
Operational limit	Any operating limit
Privileges	Officer in charge of engineering watch in a manned engine room or designated duty engineer in a periodically unmanned engine room
Prior certification	None
Minimum age	18
Minimum service	<p><i>Duration:</i> 12 months' combined sea service and workshop skills training, including on-board training documented in a training record book</p> <p><i>Role:</i> six months' engine room watchkeeping duties under supervision of chief engineer officer or engineer officer</p> <p><i>Training:</i> appropriate additional engineering training and accepted/approved training course</p> <p><i>Engine power:</i> 750kW main propulsion power or more</p> <p><i>Conditions:</i> service on fishing vessels outside restricted limits with 750kW or more main propulsion power is acceptable, provided six months is served as engineering watchkeeper</p> <p>Or</p> <p><i>Duration:</i> 36 months' combined workshop skills training and sea service, of which 30 months must be in engine department (including on-board training documented in a training record book)</p> <p><i>Role:</i> six months' engine room watchkeeping duties under supervision of chief engineer officer or engineer officer</p> <p><i>Training:</i> accepted/approved training course</p> <p><i>Engine power:</i> 750kW main propulsion power or more</p> <p><i>Conditions:</i> service on fishing vessels with 750kW or more main propulsion power is acceptable, provided six months is served as engineering watchkeeper</p>
Training and supplementary certificates	<ul style="list-style-type: none"> • Completion of an accepted/approved training course • STCW basic training • Advanced fire fighting • Proficiency in survival craft and rescue boats (other than fast rescue boats) • Medical first aid
Career progression	<p>To MEC 2, with 12 months' sea service aboard ships with 750kW or more main propulsion machinery, as engineer officer</p> <p>To MEC 2 <3000kW, with 12 months' sea service aboard ships with 750kW main propulsion machinery or more, as assistant engineer officer or engineer officer</p> <p>To MEC 1, with 36 months' sea service as officer in charge of engineering watch aboard ships with 750kW main propulsion machinery or more</p>

36 Marine Engineer Class 2 on ships less than 3000 kilowatts

Short name	MEC 2 <3000kW
Regulation	STCW Regulation III/3
Replaces	New certificate
Operational limit	Any operating limit
Privileges	Second engineer on ships propelled by main propulsion machinery of less than 3000kW in any operating area
Prior certification	MEC 3
Minimum service	<p><i>Duration:</i> 12 months</p> <p><i>Engine power:</i> 750kW main propulsion power or more</p> <p><i>While holding:</i> MEC 3</p> <p><i>Role:</i> assistant engineer officer or engineer officer</p>
Training and supplementary certificates	<ul style="list-style-type: none"> • STCW basic safety training • Advanced fire fighting • Proficiency in survival craft and rescue boats (other than fast rescue boats) • Medical first aid
Career progression	To MEC 2 ECE, with at least 12 months' sea service while qualified to serve as second engineer officer

37 Marine Engineer Class 2

Short name	MEC 2
Regulation	STCW Regulation III/2
Replaces	MEC 2
Operational limit	Any operating limit
Privileges	<p>To perform the functions and duties of:</p> <ul style="list-style-type: none"> • second engineer on ships powered by main propulsion machinery of any propulsion power in any operating area • chief engineer on fishing ships of any propulsion power in any operating area
Prior certification	MEC 3
Minimum age	19
Minimum service	<p><i>Duration:</i> 12 months</p> <p><i>Engine power:</i> 750kW main propulsion power or more</p> <p><i>While holding:</i> MEC 3</p> <p><i>Role:</i> engineer officer</p>
Training and supplementary certificates	<ul style="list-style-type: none"> • Completion of an accepted/approved training course • STCW basic training • Advanced fire fighting • Proficiency in survival craft and rescue boats (other than fast rescue boats) • Medical first aid
Career progression	<p>To MEC 1, with 24 months' sea service (may be reduced to not less than 12 months if sea service has been as second engineer officer)</p> <p>To MEC 2 ECE, with 12 months' sea service performing engine room watchkeeping duties as engineer officer on main propulsion machinery of 750kW or more, while holding MEC 2</p>

38 Marine Engineer Class 2 endorsed chief engineer

Short name	MEC 2 ECE
Regulation	STCW Regulation III/3
Replaces	MEC 2 endorsed chief engineer
Operational limit	Any operating limit
Privileges	<p>To perform the functions and duties of:</p> <ul style="list-style-type: none"> chief engineer on ships powered by main propulsion machinery of less than 3000kW in any operating area second engineer on ships powered by main propulsion machinery of any power in any operating limit, if holding a MEC 2 certificate or equivalent
Prior certification	MEC 2 or MEC 2 <3000kW
Minimum service	<p><i>Duration:</i> 12 months</p> <p><i>Engine power:</i> 750kW main propulsion power or more</p> <p><i>While holding:</i> MEC 2 or MEC 2 <3000kW</p> <p><i>Role:</i> engineer officer</p>
Training and supplementary certificates	<ul style="list-style-type: none"> Meet competencies outlined in STCW Regulations III/3 and III/2 STCW basic training Advanced fire fighting Proficiency in survival craft and rescue boats (other than fast rescue boats) Medical first aid
Career progression	To MEC 1, with 12 months' additional sea service while holding MEC ECE and attaining competencies required by STCW section A-III/2

39 Marine Engineer Class 1

Short name	MEC 1
Regulation	STCW Regulation III/2
Replaces	MEC 1
Operational limit	Any operating limit
Privileges	Chief engineer officer on ships powered by main propulsion machinery of any propulsion power in any operating area
Prior certification	MEC 3, MEC 2, MEC 2 ECE
Minimum service	<p><i>Duration:</i> 36 months</p> <p><i>Engine power:</i> 750kW main propulsion power or more</p> <p><i>While holding:</i> MEC 3</p> <p><i>Role:</i> officer in charge of engineering watch</p> <p>Or</p> <p><i>Duration:</i> 24 months</p> <p><i>Engine power:</i> 750kW main propulsion power or more</p> <p><i>While holding:</i> MEC 2 or MEC 2 ECE</p> <p><i>Role:</i> 12 months as second engineer officer (3000kW main propulsion power or more) or 24 months as officer in charge of an engineering watch</p>
Training and supplementary certificates	<ul style="list-style-type: none"> • Completion of an accepted/approved training course • STCW basic training • Advanced fire fighting • Proficiency in survival craft and rescue boats (other than fast rescue boats) • Medical first aid
Career progression	This is the most senior engineering officer certificate

New STCW (Manila) requirements

Ships fitted with ECDIS

Deck officers working on board ships fitted with an electronic chart display information system (ECDIS) must undergo specific education and training.

When a New Zealand certificate of competency issued in compliance with STCW Regulations II/1, II/2 and II/3 is revalidated, the seafarer needs to comply with the new STCW requirements if they wish to work on ships fitted with ECDIS after 31 December 2016.

Information about the introduction of ECDIS installations and training requirements is available on the website for Admiralty Nautical Products & Services (admiralty.co.uk).

Deck officers not meeting this requirement will have the following limitation placed on their certificate of competency:

From 1 January 2017 this certificate is not valid for service on ships fitted with ECDIS.

Deck officers may ask for this limitation to be removed when they have provided documentary evidence of having completed MNZ-approved ECDIS training.

Tanker endorsements

Separate tanker endorsements are required for oil/chemical tankers and for liquefied gas tankers. The amendments to STCW define continued professional competence for seafarers revalidating tanker endorsements under Regulation I/11 as:

- *either* approved sea service, performing duties appropriate to the tanker certificate or endorsement held, for a period of at least three months in total during the preceding five years
- *or* successfully completing an accepted/approved relevant training course or courses.

Seafarers revalidating tanker endorsements must provide evidence of approved sea service appropriate to each of the types of tanker endorsement(s) on their certificate of competency.

Leadership and management requirements

Where they did not already exist, additional education and training covering human elements, leadership and management were introduced to the training course for New Zealand certificates issued before 1 July 2013.

Seafarers meeting the current certificate of competency renewal requirements will have gained sufficient leadership and management skills and will not require further training.

High-voltage requirements

The STCW (Manila) amendments introduce a requirement for engineer officers to undergo education and training in high voltage (HV) systems at both operational and management levels. This requirement applies to all engineer officers starting training after 1 July 2013. All engineer officers who work on ships with HV systems will need to comply from 1 January 2017.

An HV system involves voltage generated and distributed at high voltage (over 1000V) or transformed to and distributed at high voltage. It does not include systems where high voltage is utilised locally (such as for ignition systems, radio transmission, radar or other navigational equipment).

For new candidates seeking engineer officer certificates, HV training requirements will be incorporated into future training programmes.

Existing engineer officers revalidating their certificates after 1 January 2012 do not have to take any further action, even if they are currently working with or intend to work with HV systems. However, they will have the following limitation placed on their certificate of competency:

From 1 January 2017 this certificate is not valid for service on ships fitted with High Voltage (over 1000V) systems.

A similar limitation will apply to new certificates issued in the future, if the applicant's training has not included HV systems.

Certificate holders may ask for this limitation to be removed when they have provided documentary evidence of having completed MNZ-approved HV training.

GMDSS (Global maritime distress and safety system)

The maritime rule requirements for renewal of a GMDSS radio operator's certificate of competency were amended in 2014 to align with the STCW sea service requirements for renewal of GMDSS certificates of competency, as follows:

- *either* 12 months' radio service in a seagoing ship fully fitted with GMDSS during the five years preceding application for renewal
- *or* three months' radio service in a seagoing ship fully fitted with GMDSS during the six months immediately prior to revalidating
- *or* satisfactorily attending and completing a GMDSS renewal course acceptable to the MNZ Director within the 12 months prior to renewal
- *or* satisfactorily passing an approved test or successfully completing approved training.

The Director of MNZ may also accept evidence of having performed functions relating to the category of GMDSS certificate that are considered to be at least equivalent to the sea service required above. These may include:

- radio technicians involved in the use, installation and testing of the full suite of GMDSS equipment
- GMDSS lecturers at MNZ training providers
- dedicated radio operators in fully GMDSS-equipped stations other than ships.

A renewed GMDSS radio operator certificate of competency is valid for a further five years, based on the anniversary of the certificate's original issue date.

Security awareness training

After 1 January 2014, seafarers who wish to work on International Ship and Port Facility Security Code (ISPS) ships should complete security awareness training and will receive a certificate of proficiency in security awareness training.

Seafarers revalidating their certificate of competency after 1 January 2014 who do not have a certificate of proficiency or endorsement in security awareness training must complete an approved security awareness training course, to enable their certificate of competency to be valid after 1 January 2017.

Industry-specific certificates

Part 35 of the maritime rules provides for industry-specific training and issue of commercial certificates where the requirement for crew of a ship to hold a MNZ certificate of proficiency or competency is disproportionate to the nature and scope of operation (for example, regional council staff operating a small boat to take water samples).

Industry organisations need to apply to the Director of MNZ for approval to provide training and issue certificates if they consider their operations are appropriate for industry-specific certificates. The application must include a training framework that reflects the organisation's maritime operation.

Industry-specific certificates are *not* maritime documents and can only be issued for the operation of:

- boats of 6m or less overall
- non-passenger boats (except fishing boats) of 15m or less that operate only within restricted limits.

MNZ annually audits the organisations that are approved to train and issue industry-specific certificates, to ensure the training continues to meet the requirements set out in the approved training framework and that any requirements to maintain ongoing proficiency of certificate holders are followed.

Training and examinations

MNZ's education and training framework has been developed to ensure New Zealand-trained and certified seafarers have the required competencies, skills, and experience to meet New Zealand education standards and the international obligations and standards for seafarer training and competence (STCW and STCW-F).

The graphic on the following spread shows how the regulatory framework interacts with the training, assessment and certification process.

Regulatory framework

The new education and training framework lifts the level at which MNZ provides regulatory oversight of training and examination to complement, rather than duplicate, the accountabilities and responsibilities in the maritime education sector of other government agencies' and recognised bodies. In particular, this relates to the role of the New Zealand Qualifications Authority (NZQA) and Competenz (the industry training organisation and standard-setting body for the maritime sector).

The new framework provides for a more integrated, whole-of-government approach that will ensure more effective and seamless oversight of the training and examination of seafarers, consistent with international obligations. Each agency's responsibilities are clearly defined and reflect their respective statutory responsibilities, capabilities and capacities.

Who does what?

The key organisations involved in New Zealand seafarer training and examination are:

- government, as represented by MNZ and NZQA
- the International Maritime Organization (IMO)
- Competenz, the industry training organisation and standard-setting body⁴ for the maritime sector
- training providers – polytechnics/institutes of technology (ITPs) and private training establishments (PTEs).

The relationship between the MNZ certification process, regulatory framework and New Zealand education and training framework is outlined in the chart below.

Competency-based training and assessment

Part 32 of the maritime rules requires seafarers to meet and maintain the [required levels of competency](#) to be issued and hold a certificate. The competencies for STCW and STCW-F certificates are incorporated by reference. Part 32 also provides the high-level competencies for national certificates, which are supported by more detailed competency frameworks provided by the Director of MNZ. For more information, refer to MNZ's website: maritimenz.govt.nz/seafarers.

⁴ Standard-setting bodies (SSBs) are responsible for the quality and credibility of the assessment standards that are submitted to NZQA for registration. The SSBs work with industry partners, professional groups and other stakeholders to develop assessment standards that are nationally recognised.

Regulatory framework for New Zealand seafarer training and examination

MNZ sets the competencies that must be met to obtain a maritime certificate and is responsible for the final assessment process for issuing certificates (final examination). The competencies are set on the basis of international obligations, sector needs and statutory requirements to ensure maritime safety.

Competenz develops and moderates maritime qualifications and standards for assessment on the basis of MNZ competencies and industry input.

Training providers develop and provide the “programmes of learning for outcomes” (training courses) to deliver the NZQF standards and MNZ competency standards.

Consistency and Qualifications Review Panel provides oversight and input into new maritime training courses being developed for NZQA approval and manages the consistency of delivery of maritime national awards. The panel has representatives from Competenz, training providers and industry, and is chaired by MNZ.

NZQA registers training providers, approves programmes and applications for consent to assess against standards, and approves qualifications and standards. It also monitors and undertakes external evaluation and review of training providers and programmes, including moderation responsibilities (with MNZ providing technical support).

On-board training is recorded in a training record book (sometimes referred to as a STaRS book) approved by the Director, which sets out specific tasks and project work based on the competencies and proficiencies that must be completed.

The competency frameworks also provide the basis for Competenz, as the recognised organisation for setting standards for the maritime sector, to develop qualifications and assessment standards, and to support training providers in developing appropriate training programmes for a specific national certificate or endorsement.

Training can be provided by public and private training providers and, in some instances (such as ancillary proficiency training), by vessel operators, through courses that have been either accepted or approved by the Director of MNZ.

Training accepted under the Education Act

Most of the training for New Zealand seafarer certificates and endorsements is provided through New Zealand's education system under the Education Act. For example, training is mostly undertaken by studying for and obtaining the relevant maritime qualifications, training scheme awards or unit standards at a public or private training provider approved or accredited by the NZQA to deliver these 'national awards' (qualification, training scheme or unit standard).

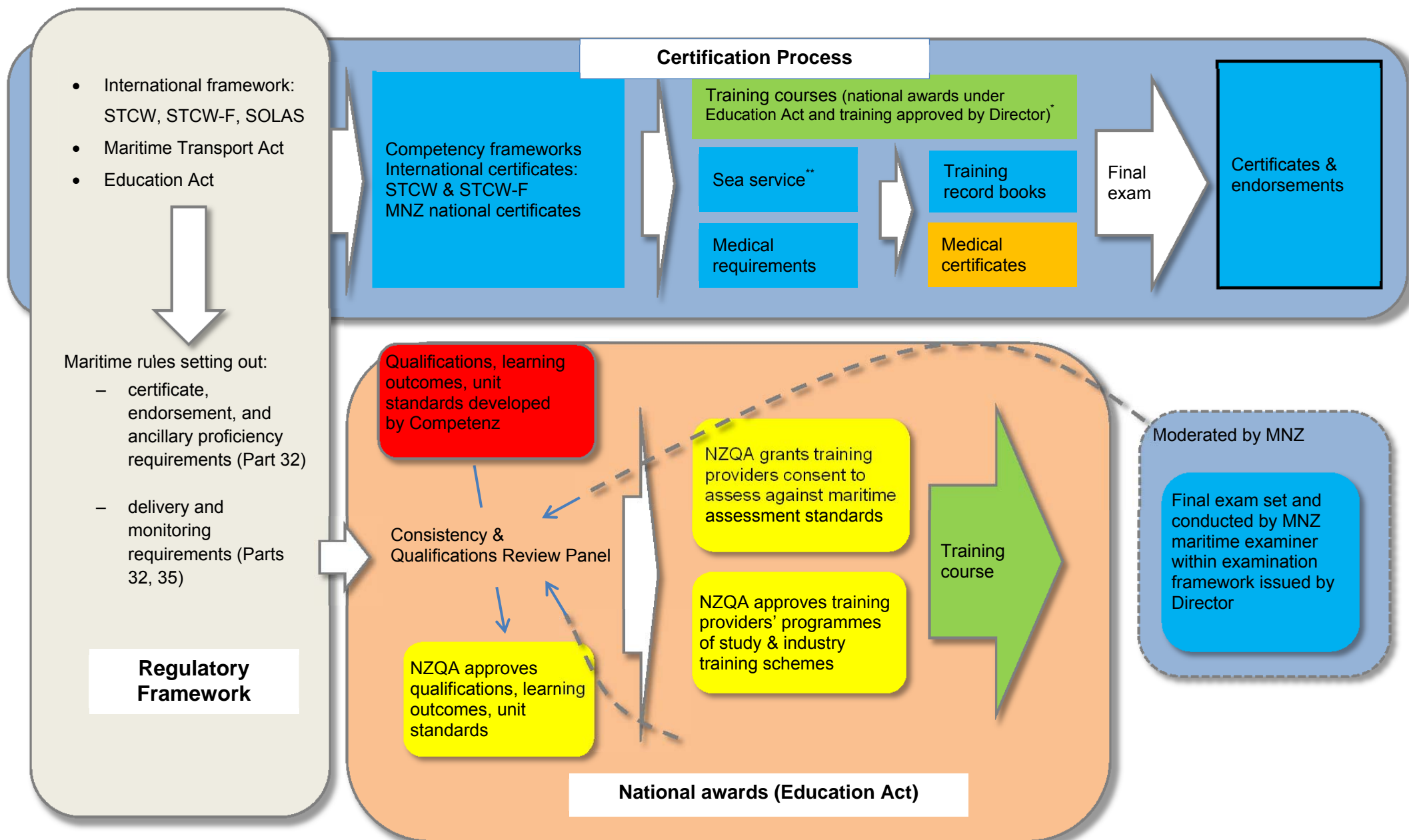
As part of the Targeted Review of Qualifications initiated by NZQA in 2011, MNZ has worked closely with industry, training providers, Competenz and NZQA to develop new maritime qualifications and unit standards that better align with MNZ's certificates, endorsements and ancillary proficiency requirements.

Training approved under Part 35

Where no national award is available under the Education Act, Part 35 of the maritime rules allows the Director of MNZ to approve training for a certificate or endorsement. This may include training delivered in combination with other training based on assessment standards provided for under the Education Act.

The Director can approve training:

- provided in combination with NZQA assessment standard-based training
- for the purpose of on-board training requirements or reduction of sea service
- for bridging courses that enable transition between or renewal of certificates
- for refreshing or updating competence or knowledge (such as changes in relevant national and international regulations, or technological changes)



* The Director of MNZ can approve courses where training for a national award is not available for a particular certificate, endorsement or ancillary proficiency.

** The Director of MNZ can approve structured training programmes for the purposes of reducing sea service requirement, or refresher training for the purposes of equivalence for renewal of certificates.

A list of the national awards acceptable to the Director, and any training approved under Part 35, can be found [here](#) and is also available on MNZ's website: maritimenz.govt.nz/seafarers. This will be regularly updated when:

- new qualifications, assessment standards and training courses acceptable to the Director of MNZ are approved by NZQA
- training is approved by the Director under Part 35.

Training providers

There are two types of training organisation offering seafarer training in New Zealand: polytechnics/institutes of technology (ITPs) and private training establishments (PTEs).

ITPs deliver technical, vocational and professional education, while PTEs are private organisations that provide education/training (that is, they are not state-owned). ITPs offer a broader range of training up to NZQF Level 3 for seafarers. PTEs generally offer training towards lower-level national seafarer certificates.

Training providers need to be registered and have their training programmes approved and accredited by NZQA. Their responsibilities include:

- developing and delivering training programmes, courses and internal assessments
- internal course assessment and testing
- maintaining a robust internal moderation regime for all internal assessment processes (required under the Education Act where assessments are against national standards, maritime rules or non-national standards)
- maintaining internal quality management systems, which includes internal moderation processes.

Final examination

A critical quality-control element of the seafarer certification system is the final examination or assessment of candidates conducted by MNZ-appointed examiners or assessors. These take place after the successful completion of the required training programmes and sea service.

The final examination is in the form of an oral examination, conducted in an appropriate location. The exam is designed to determine whether the candidate has the necessary competencies and attributes to safely carry out the duties relevant to the certificate being applied for. It is based on the competencies set out in STCW, STCW-F or the national competency framework, as appropriate.

Failure to pass the final examination or assessment means the seafarer will not be issued with a certificate *even if he or she has passed* the required training courses and has maritime qualifications or certificates under the Education Act.

Oversight of examiners and assessors

MNZ provides comprehensive oversight of the final examination system, which includes the recognition and training of examiners, review of examination results and trends, review and upkeep of examiner currency and conduct, and regular communication with examiners, including through forums and seminars.

All recognised examiners must have experience and training appropriate to the examinations they conduct. They are guided by examination frameworks provided by MNZ. These frameworks contain a range of topics, based on a certificate's competency requirements. The candidate is expected to demonstrate satisfactory knowledge and competence according to the certificate grade, although the procedure and result of the examination is at the examiner's discretion.

Maritime examiners and assessors may be employed by a training provider and conduct internal assessment for their training courses. However, they must not have had an academic relationship with the seafarer being examined.

Medical fitness requirements

A career at sea requires a seafarer to be medically fit. It is better to identify any problems with medical fitness early on, and strongly recommended that a certificate of medical fitness is obtained before training begins.

Colour vision is an important aspect of medical fitness for seafarers. Ships' navigation lights and aids to navigation (beacons and buoys) are coloured, as are warning lights on control panels and electrical wiring.

It is possible for a person to have problems with their colour vision without being aware of them. Having colour vision tested early is a wise precaution before starting seafarer training.

A certificate of medical fitness is required if a seafarer:

- is under 18 and the ship operates outside restricted limits
- has an MNZ certificate of proficiency or competency
- works on foreign-going passenger ships, foreign-going non-passenger ships of 500GT or more
- works on New Zealand passenger and non-passenger ships of 45m or more that proceed outside restricted limits.

Certificates of medical fitness are valid for two years, after which they must be renewed. There is also an obligation for the certificate to be 'current', which means a seafarer who suffers a serious illness or injury or develops a new medical condition must obtain a new certificate before returning to work.

Medical examinations

Medical examinations for STCW and STCW-F-aligned certificates need to be carried out by a medical practitioner approved by the Director of MNZ. An approved medical practitioner must be registered and have experience relevant to assessing that seafarers are fit for the types of duties they are likely to undertake at sea. The medical practitioner must be professionally independent from employers, seafarers, and employer and seafarer representatives.

A list of approved medical practitioners is available on MNZ's website: maritimenz.govt.nz/seafarers.

Certificates of medical fitness for national certificates may be issued by a registered medical practitioner (and do not need to be issued by a medical practitioner approved by the Director).

A seafarer working on a ship operating in the unlimited area must also carry a certificate showing the type, place and date administered of all vaccinations they have received.

Medical standards

When conducting the assessment, the medical practitioner needs to consider a range of matters before issuing a certificate of medical fitness. These include the requirements and standards specified in Appendices B to E of the International Labour Office/IMO *Guidelines on the Medical Examinations of Seafarers* (available on MNZ's website at maritimenz.govt.nz), vaccination requirements for seafarers operating in unlimited areas, and the nature of the duties the seafarer performs.

Eyesight and colour vision standards

The medical practitioner must ensure that the seafarer's eyesight and colour vision meet the eyesight and colour vision standards set out in STCW A-I/9. These standards apply for all New Zealand certificates of proficiency and competency. The seafarer must also be examined for any evidence of serious or progressive eye disease.

The application of the standards varies according to the duties and functions (for example, deck or engineering) and, in the case of national certificates, the area of operation. The options are outlined in the table below.

Certificate type	General eyesight standards	Colour vision standards
STCW and STCW-F deck	As prescribed in A-I/9 of STCW code Must pass general eyesight test in accordance with this standard	As prescribed in A-I/9 of STCW code Must pass colour vision test in accordance with this standard
STCW and STCW-F radio, engineering and electro-technical	As prescribed in A-I/9 of STCW code Must pass a general eyesight test subject to allowance for combined vision in standard	As prescribed in A-I/9 of STCW code Restricted certificate permitted if test failed, as considered appropriate by the Director
National deck	As for STCW and STCW-F deck	As for STCW and STCW-F deck Restricted certificate limiting to daylight hours only may be issued if test failed
National engineering	As for STCW and STCW-F engineering	No standard to meet
Others	Sufficient for duties	Sufficient for duties

Table 1 Eyesight and colour vision standards for New Zealand seafarers

Failing eyesight and colour vision tests does not prevent a medical practitioner from issuing a medical certificate of fitness. However, the medical practitioner must indicate on the certificate any restrictions or limitations that apply.

The Director of MNZ can also require a seafarer to undergo eyesight and vision tests conducted by a registered optometrist for the issue of a certificate of medical fitness.

Categories of medical certificate

The medical practitioner must issue a certificate of medical fitness in the form specified by the Director of MNZ, indicating which medical category applies to the seafarer, and then forward a copy of the certificate of medical fitness to the Director.

National certificates

The medical practitioner has a greater degree of discretion when considering medical categories for seafarers who are applying for, or hold, national certificates, to take account of:

- the proximity of ships the seafarer can work on to the shore and to shore-based medical facilities
- how the proximity to shore may lower any risk of adverse effects to the health and safety of the seafarer, other seafarers and passengers, or the safety of the ship. For example, the availability of shore-based medical care and the time it may take a seafarer to receive it (such as SRL holders working in enclosed limits), how readily a condition can be effectively monitored by a health professional, limiting duties to those where the medical condition is not relevant, and so on).

Code	Name	Conditions	Implication
A	Unrestricted sea service	None	Seafarer is fully fit for unrestricted sea service and the expiry date should therefore be the maximum allowed
A(T)	Unrestricted sea service	Medical surveillance required at intervals Certificate is valid until the time such surveillance is required	Seafarer needs medical supervision, which should be specified. The certificate's period of validity is up to the medical practitioner's discretion and may range from the maximum downwards. There may also be restrictions on the nature or area of allowable sea service
B	Restricted sea service only	Restrictions endorsed on the medical certificate	These may be restrictions such as 'restricted to NZ coastal waters only'. The certificate's period of validity is up to the medical practitioner's discretion and will range from two years downwards
C	Temporarily unfit for sea service	Endorsed with number of weeks before the seafarer is re-examined	Seafarer is considered temporarily unfit for sea service. Certificate must state the length of time (weeks) before or the date after which the seafarer may be re-examined
D	Indefinitely unfit for sea service	Endorsed with number of months before the seafarer is re-examined	Seafarer is considered indefinitely unfit for sea service. The certificate must state the length of time (months) or conditions to be met before the seafarer can be re-examined
E	Permanently unfit for sea service	Cannot work at sea	Seafarer is considered permanently unfit for sea service

Table 2 Categories of medical fitness for New Zealand seafarers

No operator or master may employ a seafarer, or seafarer join a ship, unless they have a valid category A, category A(T), or category B certificate of medical fitness, as appropriate to the position.

More detailed guidance about the medical fitness requirements can be found in the Advisory Circular to Part 34, Guidance for Medical Practitioners and Guidance for Optometrists, which are available on the [MNZ website](#).

Part 3 Renewal

From 2014, nearly all certificates need to be kept current through renewal (or revalidation, as previously known).

Renewal is currently required only for LLO and STCW certificates. This requirement – which includes medical fitness – has been extended to certificates for:

- skippers, masters and deck and engineer officers on passenger and non-passenger vessels operating in enclosed and inshore limits (restricted limits)
- skippers and engineers on non-passenger and fishing vessels of less than 24m in the coastal and offshore areas
- mates, skippers and engineers of fishing vessels 24m and more in coastal, offshore and unlimited waters.

Qualified Deck Crew and Advanced Deckhand – Fishing certificates of proficiency do not need to be renewed, but evidence of medical fitness for duty is required every five years to renew the certificate.

Renewal of national certificates

Renewal is required every five years for restricted-limits certificates and for coastal/offshore certificates for vessels of less than 24m.

Current certificate	New certificate
Local Launch Operator ⁵ ; Inshore Launch Master	Skipper Restricted Limits
New Zealand offshore watchkeeper with ILM endorsement	Skipper Coastal/Offshore
None	Master Yacht <24m
Marine engineer class 4	Marine Engineer Class 4
Marine engineer class 5	Marine Engineer Class 5
Marine engineer class 6	Marine Engineer Class 6

Table 3 Renewal of national certificates

Renewal of certificates for restricted limits and coastal/offshore <24m and for fishing certificates ≥24m is required five years after the issue of that certificate, under the amended rules and transition provisions, these qualifications will not require renewal until at least 2019.

Sea service

Evidence of at least *six months' approved sea service* within the previous five years, or 45 days in the three months immediately before renewing, must be provided when applying to renew.

5 Local launch operator (LLO) certificates are already required to be revalidated every five years. When holders of an LLO are required to transition to a Skipper Restricted Limits certificate, this five-yearly renewal cycle will be unaffected.

Alternatives to sea service

A *practical competency assessment* is available if a seafarer does not meet the sea service requirements in restricted or coastal/offshore limits, or if the certificate has expired. Depending on this assessment, further training may be needed.

Alternatively, the seafarer can undertake a *course and assessment* in the relevant rules concerning safety management systems, the safety of life at sea and protection of the marine environment.

Equivalent renewal experience

The Director of MNZ is also able to accept experience in certain non-seagoing positions as equivalent to sea service when renewing non-STCW certificates, if the position has been held for at least two and a half years within the previous five years.

The following occupations may be considered as equivalent sea service for renewing non-STCW certificates:

- lecturers with approved maritime training providers
- technical, engineering or marine management positions
- MNZ maritime officers
- Maritime Operator Safety System (MOSS) surveyors
- harbourmasters.

Medical fitness

Evidence must be provided of medical fitness, including eyesight, hearing and speech. The medical examination must be carried out by a registered medical practitioner.

Ancillary certificates

There is no requirement for the basic safety training or any other ancillary certificates to be revalidated. However, it is strongly recommended that first aid training is kept current.

Renewal of fishing (STCW-F) certificates

SeaCert has aligned the fishing certificates for unlimited vessels and vessels $\geq 24\text{m}$ in the coastal and offshore limits with the requirements of STCW-F.

Renewal will be required every five years for certificates as skippers, officers and engineering officers aboard commercial fishing vessels $\geq 24\text{m}$ in coastal and offshore limits (STCW-F – limited waters) and the unlimited area (STCW-F – unlimited waters).

Current qualification	New qualification
New Zealand offshore watchkeeper	Mate Fishing Vessel – Limited
New Zealand offshore master	Skipper Fishing Vessel – Limited
Mate deep sea fishing vessel	Mate Fishing Vessel – Unlimited
Master deep sea fishing vessel	Skipper Fishing Vessel – Unlimited

Table 4 Renewal of fishing qualifications

Sea service

The seafarer should provide evidence of having completed:

- either at least one year's sea service as skipper, mate or engineer officer during the preceding five years
- or approved sea service as a mate for at least three months on a fishing vessel in a supernumerary capacity (that is, a worker who is additional to the vessel's crewing requirements) immediately before taking up the position for which the certificate is valid.

Alternatives to sea service

If the seafarer has not gained enough sea service during the five-year period, it may be possible to pass an approved test, or successfully complete an approved course appropriate for skippers and officers serving aboard fishing vessels – especially for re-entrants to sea service on fishing vessels.

Equivalent renewal experience

The Director of MNZ can accept fishing vessel operational duties ashore, relating to the duties appropriate to the grade of certificate held, if they are considered to be at least equivalent to sea service as skipper or officer aboard fishing vessels.

The Director may revalidate an STCW-F certificate if a holder has completed at least two and a half years' non-seagoing experience in the previous five years in an equivalent fishing vessel operational position ashore. Such positions could include:

- lecturers with approved fishing training providers
- marine and technical managers at companies operating STCW-F-sized fishing vessels (≥24m).

Medical fitness

Evidence must be provided of medical fitness, including eyesight, hearing and speech. The medical examination must be carried out by an MNZ-approved medical practitioner.

Ancillary certificates

There is no requirement for basic or advanced safety training to be renewed. However, it is strongly recommended that any first aid or medical training is kept current, and you may wish to consider maintaining any STCW ancillary proficiency training.

Renewal of STCW certificates

The STCW Convention 1978 was updated by the 2010 Manila amendments and contains new requirements for all seafarers holding STCW certificates.

Date	Action
1 January 2012	Manila amendments entered into force and may be applied by MNZ
1 July 2013	New entrants starting training must satisfy new Manila provisions and all training programmes must meet Manila requirements
1 January 2014	Security training must match Manila amendments
1 January 2017	All certificates must meet Manila requirements. No STCW-95 certificates will be accepted

Table 5 Transition timetable for renewal of STCW certificates

Seafarers renewing certificates of competency (certificate of competencies) will be required to submit additional evidence to ensure their certificate is valid for service on certain types of ships after 31 December 2016.

Sea service

Before a certificate can be renewed, the seafarer needs to show continued professional competence by providing evidence of approved sea service, performing functions appropriate to the certificate held, for a total of:

- *either* 12 months during the preceding five years
- *or* three months in the six months immediately before renewal.

Alternatives to sea service

If insufficient sea service has been gained during the five-year period, an STCW certificate may be renewed by:

- passing an approved test
- or successfully completing an accepted/approved training course or courses
- or completing approved sea service and performing functions appropriate to the certificate held for a period of at least three months in a supernumerary capacity (additional to the vessel's crewing requirement), or in a lower officer rank than the one the certificate is valid for, immediately before taking up the new rank.

Equivalent renewal experience

STCW allows for professional competence for sea service to be established by having performed functions ashore considered to be equivalent to the above sea service.

The Director may renew an STCW certificate if a holder has completed at least two and a half years' non-seagoing experience in the previous five years in a position considered by the Director to be equivalent to sea service. These may include:

- harbour or coastal pilots
- harbourmasters
- marine college lecturers
- technical, engineering and marine superintendents or ship repair managers
- IACS (International Association of Classification Societies) surveyors.

Medical fitness

Evidence must be provided of physical and medical fitness, including eyesight and hearing, to the standards included in the STCW (Manila) amendments. This evidence must be a valid seafarer certificate of medical fitness.

Validity periods

If an application for renewal of a certificate of competency is made within the six months before the certificate expiry date, the certificate will be renewed until the fifth anniversary of its expiry date.

Certificates received outside this six-month period will be renewed for five years from the date of renewal.

Ancillary certificates

From 1 January 2017, seafarers who are required under STCW to hold the following ancillary certificates must provide evidence every five years of having maintained the required standards of competence to undertake the required tasks, duties and responsibilities:

- STCW basic training (personal survival techniques, and fire prevention and fire fighting)
- proficiency in survival craft and rescue boats (other than fast rescue boats)
- proficiency in fast rescue boats
- advanced fire fighting.

Seafarers renewing their certificate of competency *after 1 January 2017* must submit documentary evidence of having completed MNZ-approved updating/refresher training, where required. This requirement does not apply to seafarers applying for a certificate of competency renewal *before 1 January 2017*.

The following certificates do not need to be renewed:

- the personal safety and responsibility and elementary first aid components of STCW basic safety training
- proficiency in medical first aid on board ship
- proficiency in medical care on board ship
- proficiency for ship security officer.

However, it is strongly recommended that any first aid or medical care training is kept current.

Evidence of on-board training and experience

MNZ may accept on-board training and experience as meeting the requirements for maintaining the standard of competence. The competencies for each ancillary certificate are defined in the appropriate tables in Chapter VI of the STCW code and set out below. Records should be kept of this on-board training and experience.

Other competencies need to be demonstrated through evidence of completion of an approved training/refresher course.

Competence	On-board training and experience	Training course
Survive at sea in the event of ship abandonment	Don a lifejacket Board a survival craft from the ship while wearing a lifejacket Take initial actions on boarding a lifeboat to enhance chance of survival Stream a lifeboat drogue or sea anchor Operate survival craft equipment Operate location devices, including radio equipment	Don and use an immersion suit Safely jump from a height into the water Right an inverted liferaft while wearing a lifejacket Swim while wearing a lifejacket Keep afloat without a lifejacket

Table 6 Renewal requirements for personal survival techniques

Competence	On-board training and experience	Training course
Fight and extinguish fires	<p>Use self-contained breathing apparatus</p> <p>Effect a rescue in a smoke-filled space, using an approved smoke-generating device aboard, while wearing a breathing apparatus</p>	<p>Use various types of portable fire extinguishers</p> <p>Extinguish smaller fires such as electrical, oil or propane fires</p> <p>Extinguish extensive fires with water, using jet and spray nozzles</p> <p>Extinguish fires with foam, powder or any other suitable chemical agent</p> <p>Enter and pass through, with lifeline but without breathing apparatus, a compartment into which high-expansion foam has been injected</p> <p>Fight fire in smoke-filled enclosed spaces wearing self-contained breathing apparatus</p> <p>Extinguish fire with water fog or any other suitable fire-fighting agent in an accommodation room or simulated engine room fire and heavy smoke</p> <p>Extinguish oil fire with fog applicator and spray nozzles, dry chemical powder or foam applicators</p>

Table 7 Renewal requirements for fire prevention and fire fighting

Competence	On-board training and experience	Training course
Take charge of a survival craft or rescue boat during and after launch	<p>Interpret markings on survival craft as to the number of persons they are intended to carry</p> <p>Give correct commands for launching and boarding survival craft, clearing the ship and handling and disembarking persons from survival craft</p> <p>Prepare and safely launch survival craft and clear the ship's side quickly</p> <p>Safely recover survival craft and rescue boats</p> <p>Use portable radio equipment for survival craft</p>	Right an inverted liferaft while wearing a lifejacket
Manage survivors and survival craft after abandoning ship	<p>Row and steer a boat, and steer by compass</p> <p>Use individual items of equipment of survival crafts, except for pyrotechnics</p> <p>Rig devices to aid location</p>	
Use locating devices, including communication and signalling apparatus	Use portable radio equipment for survival craft	
Apply first aid to survivors		

Table 8 Renewal requirements for proficiency in survival craft and rescue boats (other than fast rescue boats) (PISC)

Competence	On-board training and experience	Training course
Take charge of a fast rescue boat during and after launch	<p>Control safe launching and recovery of a fast rescue boat</p> <p>Handle a fast rescue boat in prevailing weather and sea conditions</p> <p>Use communications and signalling equipment between fast rescue boat and a helicopter and a ship</p> <p>Carry out search patterns, taking account of environmental factors</p>	<p>Right a capsized fast rescue boat</p> <p>Recover a casualty from the water and transfer a casualty to a rescue helicopter or to a ship or a place of safety</p>

Table 9 Renewal requirements for proficiency in fast rescue boats

Competence	On-board experience and training	Training course
Control fire-fighting operations aboard ships	<p>Use fire-fighting procedures at sea and in port, with particular emphasis on organisation, tactics and command</p> <p>Manage communication and coordination during fire-fighting operations</p> <p>Control ventilation, including smoke extraction</p> <p>Control fuel and electrical systems</p> <p>Manage fire-fighting process hazards (dry distillation, chemical reactions, boiler uptake)</p> <p>Fire precautions and hazards associated with the storage and handling of materials</p> <p>Management and control of injured persons</p> <p>Procedures for coordination with shore-based fire fighters</p>	<p>Use of water for fire extinguishing, the effect on ship stability, precautions and corrective procedures</p> <p>Fire fighting involving dangerous goods</p>

Table 10 Renewal requirements for advanced fire fighting

Part 4 Minimum safe crewing

Part 31 of the maritime rules sets out the requirements for minimum crew numbers and crew certificates and qualifications for New Zealand passenger and non-passenger commercial ships and fishing vessels. It also provides the process for issuing minimum safe crewing documents (MSCD) and the requirements for fitness for duty, prescribed hours of rest and watchkeeping.

The rule has four core subparts, summarised in Table 11 below.

Requirements to hold certificates

The crew positions required in the MSCD or specified in the tables and flowcharts in Part 31 may only be filled by seafarers who hold the certificates or proficiencies identified for those positions. The only exceptions to this are where a crew member holds an appropriate higher-grade certificate with related privileges that are the same as the privileges of the required certificate, or a foreign certificate recognised by the Director of MNZ.

The owner of a ship that is required to have a MSCD must prepare a proposal for the minimum safe crewing level for the ship and submit it to the Director. The assessment needs to cover crew numbers and crew certification. The proposal is then assessed by MNZ technical staff, who take into account the particular circumstances of the vessel, equipment, crew, passengers, weather, operating area, length of voyage, shore support and other vessel support.

Once issued, the MSCD is valid for five years (unless the ship undergoes a change in trading or construction, machinery, equipment, operation or maintenance that affects the minimum safe crewing level), and must be displayed on the ship at all times.

Engineering systems

The flowcharts in Part 31 and accompanying advisory circular refer to systems that have been developed with industry to determine engineer crewing requirements.

'System' means any system an engineer is responsible for, other than the main propulsion machinery. Examples of such systems are:

Bilge system with oily water separator: where the bilges are normally and routinely pumped overboard through the oily water separator (when it is legal to do so). If the vessel's normal operational pattern involves pumping bilge water into a holding tank and then ashore – even though an oily water separator may be fitted – this does not count as a system.

Purifiers and oil fuel transfer systems: the fuel is stored in two or more storage tanks and there is pipework and pumping capacity to enable it to be transferred between storage tanks for reasons of stability. For consumption, the fuel is pumped from the storage tanks to a settling tank, and then through a purifier to a daily service tank or tanks supplying the main and auxiliary engines.

Boiler and any associated heating system: an oil-fired boiler supplying steam under pressure for tank or accommodation heating, or for process work in an on-board factory area.

Automatic sprinkler system: a fire-fighting sprinkler system protecting the passenger or crew accommodation or cargo spaces, set off automatically by fire/smoke detectors in the event of fire.

Cold chambers with separate refrigerating plant: cargo spaces for the carriage of chilled or frozen cargo, with a central refrigeration plant supplying all spaces. This does not include chambers or plant solely for carriage of supplies for crew and passengers.

Water ballast and transfer system: two or more tanks and associated pipework and pumping systems enabling ballast water to be routinely pumped into or out of any tank or tanks, or transferred between tanks, as a means of controlling a vessel's stability.

Computer-controlled machinery: main and auxiliary machinery arranged so that all functions (starting, controlling and stopping engines, pumps and other machinery) can be automated under the control of a central computerised system.

Auxiliary generators capable of operating in parallel: two or more generators or alternators able to be connected in parallel to run the electrical systems through a central switchboard. This does not apply if two or more generators each supply electrically separate parts of the system, or if it is not possible to connect more than one generator to the switchboard at a time.

Reticulated hydraulic system: one or more hydraulic pumps supplying more than one hydraulic motor from a central location using a hydraulic ring-main.

Transverse thrusters: where these are driven by mechanical, electrical or hydraulic means (does not include azimuthing or Voith-Schneider main propulsion machinery).

More detailed information about minimum safe crewing, navigational and watchkeeping requirements and managing fatigue can be found on [MNZ's website: maritimenz.govt.nz](http://maritimenz.govt.nz).

General crewing and other requirements	SOLAS and STCW ships	STCW-F ships (fishing ships >24m in coastal and offshore limits, and fishing ships in unlimited waters)	All other ships ⁶
Owners and masters of all ships to assess crew numbers and qualifications needed to operate safely and must ensure those numbers and qualifications are on board. This must then be monitored	Tables of watchkeeping numbers and qualifications to be complied with plus necessary non-watchkeeping crew	Tables of total crew numbers and qualifications to be complied with	Tables of total crew numbers and qualifications to be complied with for: <ul style="list-style-type: none"> ✓ fishing ships: <ul style="list-style-type: none"> • less than 24m in coastal and offshore limits • in inshore fishing limits and restricted limits ✓ non-passenger ships less than: <ul style="list-style-type: none"> • 24m in coastal and offshore limits • 500GT in restricted limits ✓ passenger ships carrying fewer than: <ul style="list-style-type: none"> • 50 passengers in inshore limits • 100 passengers in enclosed water limits
	Must hold MSCD issued by Director on basis of minimum safe crewing assessment	No MSCD required	Must hold MSCD issued by Director on basis of minimum safe crewing assessment: <ul style="list-style-type: none"> ✓ passenger ships: <ul style="list-style-type: none"> • less than 24m in coastal and offshore limits • carrying 50 or more passengers in inshore limits • carrying 100 or more passengers in enclosed water limits ✓ ships between 500 and 3000GT operating exclusively in restricted limits ✓ a high-speed craft to which section 2 of Part 40A applies ✓ ships handling harbour tugs ✓ pilot ships

Table 11 Summary of minimum safe crewing requirements

⁶ The full requirements of STCW and STCW-F for propulsion power are not applied for vessels of less than 24m operating in coastal and offshore limits. The propulsion power of vessels less than 24m is based on the power in kilowatts of the single largest propulsion engine. For vessels that require STCW or STCW-F engineering certificates, propulsion power is the total continuous rated output power, in kilowatts, of all the ship's main propulsion machinery, that appears in the ship's certificate of registry or other official document.

Part 5 Transitional arrangements

Operational limits

Ships assigned coastal limits that are within the area of the new coastal limits are deemed to have the new coastal limits. However, ships assigned coastal limits that go beyond the area of the new coastal limits and ships assigned coastal limits continue to have the original limits until the certificate of survey is renewed.

In all cases, the operator must continue to comply with any safety or survey requirements that applied under the previous rules, including any conditions or limitations stated on their certificate of survey.

Ships that have been assigned enclosed, inshore or offshore limits, or the unlimited area under Part 20 before it was revoked, are deemed to have the same limit under the new Part 20.

Fishing ships that have been assigned limits of “restricted coastal – restricted to the territorial sea” are deemed to have the new inshore fishing limit.

Moving to the new certificates

From 1 April 2014, old and existing maritime certificates start being replaced by certificates in the new seafarer certification system. All seafarers who want to keep using their certificates will:

- *either* have their certificate carried over or deemed as a new certificate
- *or* need to transition to a new certificate.

Carried over and deemed certificates

If a new certificate is the same or is deemed to be the same as an existing certificate in the previous Part 32 (in place prior to 31 March 2014), the existing certificate is confirmed as the new certificate (see Table 12 below).

If a seafarer holds one of these certificates, he or she will be issued with the new certificate when the existing certificate is renewed – provided they meet the renewal requirements for the new certificate. In some instances, such as for STCW certificates, ancillary training will need to be kept up to date.

If your current certificate has no expiry date, the confirmed certificate will remain valid until five years after the amended rules come into force, and then the certificate will need to be renewed.

Old certificate/endorsement	Deemed new certificate/endorsement	Category
Able seafarer deck	Able Seafarer Deck	STCW
Able seafarer engine	Able Seafarer Engine	STCW
Electro-technical rating	Electro-technical Rating	STCW
Electro-technical officer	Electro-technical Officer	STCW
New Zealand chief mate (yacht)	Chief Mate Yacht	STCW
New Zealand master (yacht less than 500GT)	Master Yacht <500GT	STCW
New Zealand master (yacht)	Master Yacht	STCW
Chief mate of foreign-going ship	Chief Mate	STCW
Master of foreign-going ship (master mariner)	Master	STCW
Officer in charge of navigational watch of	Watchkeeper Deck	STCW

foreign-going ship		
Master of foreign-going ship <3000GT	Master on Ships <3000GT unlimited	STCW
Deck watch rating	DWR	STCW
Marine engineer class 1 (MEC 1)	MEC 1	STCW
Marine engineer class 2 endorsed chief engineer (MEC 2 ECE)	MEC 2 ECE	STCW
Marine engineer class 2 (MEC 2)	MEC 2	STCW
Marine engineer class 3 (MEC 3)	MEC 3	STCW
Engine room watch rating	EWR	STCW
NZOM STCW-95	Master < NC	STCW
ADH-F	ADH-F	STCW-F
Mate of deep-sea fishing vessel	MFV-U	STCW-F
Master of a deep sea fishing vessel	SFV-U	STCW-F
CDH, ADH	QDC	National
Marine engineer class 6	MEC 6	National
Marine engineer class 5 motor	MEC 5	National
Marine engineer class 5 steam	MEC 5 steam	National
Marine engineer class 4	MEC 4	National
Integrated rating confirmed by MNZ Director as meeting requirements of STCW Regulations II/6 and III/6	DWR endorsed with EWR	STCW
Tanker endorsement	Tanker endorsement of relevant kind under this Part (limited to appropriate type of tanker)	STCW
High-speed ship endorsement	High-speed ship endorsement	National

Table 12 Carried over and deemed certificates

Transition

A small number of certificates can transition to more than one new certificate (see Table 13 below). Seafarers who hold these certificates will be able to apply for the certificate that most appropriately reflects their experience, training and sea service as soon as the new rules come into effect.

If your existing certificate already needs to be renewed every five years (for example, LLO), you must apply for the new certificate when the old certificate expires.

If the existing certificate has no expiry date, you will have up to 31 March 2019 to apply for a new certificate. After this time, your certificate will become invalid for the purposes of maritime rules relating to crewing and manning. Until then, if you hold one of these certificates you can continue to operate on New Zealand ships in the same capacity as you currently do, until you apply for the new certificate or until 2019, whichever comes first.

Exceptions to this apply to seafarers holding an NZOM or NZOW and operating as master of a vessel less than 500GT in the coastal area or as officer of the watch on a vessel less than 500GT in the near-coastal area, respectively. In these cases, certificates will be replaced by an STCW or STCW-F certificate. NZOM and NZOW holders will no longer be able to operate in these capacities after 31 December 2016 without meeting the STCW ancillary requirements of the equivalent new certificate.

Former Part 32 certificate	Possible new certificate
NZOW with ILM endorsement	SRL endorsed to 24m
	SRL endorsed to 24m–500GT
	SCO
ILM	SRL endorsed to 24m with or without endorsements (passenger, 500GT)
LLO	SRL with or without endorsements (passenger, 24m, 500GT)
NZOM	Master <500GT NC (STCW) limited to coastal limit
	SFV (STCW-F)
NZOW	MFV (STCW-F)
	Watchkeeper Deck <500GT NC (STCW)

Table 13 New certificate options for some former Part 32 certificates

NZOM and NZOW certificate holders will be able to apply for both possible new certificates identified in Table 13. They will be issued with separate certificates aligned to STCW and STCW-F if they can demonstrate they have relevant experience and ancillary proficiencies (that is, in fishing ships for the STCW-F certificate and in non-fishing ships (passenger and non-passenger ships) for STCW certificates).

Legacy certificates

SeaCert equivalents have also been identified for legacy certificates. However, these may not be directly confirmed as an equivalent new certificate because of significant developments in areas including ship operation, technology and environmental performance since these certificates were issued.

Many of these certificates were also issued in perpetuity, and MNZ knows little about holders' current competencies to exercise the privileges of the identified equivalent certificate in SeaCert.

The application for transition will require seafarers to demonstrate that they have undertaken and kept current the additional proficiencies and competencies needed to hold the equivalent certificate in SeaCert. For example, holders of STCW 78 certificates will be required to meet STCW-95 requirements before being eligible for transition to an STCW 78, as amended (Manila) certificate.

People transitioning from skipper of a coastal fishing boat, New Zealand coastal master or master small home trade ship will have their new certificate restricted to operating 100NM from the coast, to reflect existing privileges.

Transition options for legacy certificates

Legacy certificates subject to transition	Possible certificate	Category
Mate foreign-going	Chief mate	STCW
Class 2 deck officer		
Master foreign-going	Master	STCW
Class 1 deck officer		
Second mate foreign-going	Watchkeeper Deck	STCW
AB	AB Deck	STCW
AB certificate (ILO)		
Chief engineer	MEC 1	STCW
First class engineer		
Second engineer	MEC 2	STCW
Second class engineer		
NZ coastal master	Master < NC restricted to 100NM from coast	STCW
QFDH	ADH-F	STCW-F
Mate of deep-sea fishing boat	MFV-U	STCW-F
Mate home trade		
Skipper of deep-sea fishing boat	SFV-U	STCW-F
Master of foreign-going fishing boat		
ADH, CDH	QDC	National
Second class diesel trawler engineer	MEC 6	National
Engineer restricted limits motor ship		
PVOS		
Second class coastal motor	MEC 5	National
Marine engine watchkeeper		
Marine engineer class 3 (CI3)		
River engineer		
Engineer, local ship		
Engineer local motor ship		
Third class steam		

First class coastal motor	MEC 4	National
First class diesel trawler engineer		
Master river ship holding an engineering qualification	SRL endorsed to 24m	National
	SRL endorsed to 24m	National
	SCO	National
Local launchman's licence Master of restricted limit launch Commercial launchmaster ⁷ Inshore fishing skipper	SRL endorsed to 24m with or without national certificate endorsements (passenger)	National
	SRL with or without national certificate endorsements (passenger, 24m)	National
Skipper of coastal fishing boat NZ coastal master Master small home trade ship	Master <500GT NC limited to 100NM from coast	STCW
	SFV limited to 100NM from coast	STCW-F
Master river ship	MFV (STCW-F)	STCW-F
	Watchkeeper deck <500GT NC	STCW

Table 14 Transition options for legacy certificates

Transition considerations for legacy certificates

The transition provisions for legacy certificates recognise that legacy certificate holders usually have many years' experience exercising the privileges associated with their certificate. In particular, the provisions recognise that this experience has given these seafarers the familiarity and competence in recent developments in ship operation, technology and environmental performance required by the equivalent new certificate.

For this reason, the Director has discretion to determine how he will be satisfied that the legacy certificate holder meets the competency requirements for the new certificate. This includes:

- whether the competency standard can be demonstrated through a combination of prior training and sea service considered by the Director as appropriate and adequate for the certificate
- giving appropriate weight to the value of instruction and practice in the course of sea service and relevant experience as an alternative way that relevant competency may have been achieved or maintained
- considering sea service served over the seafarer's entire career (although experience in the past 10 years will be given greater weight).

The Director also has the ability to restrict the new certificate if it is not possible for the seafarer to meet all the relevant competency standards for the new certificate but the Director is satisfied that this can be adequately addressed by restricting the privileges.

Where there is more than one certificate or prescribed endorsement, the Director may issue the certificate they consider most appropriate.

⁷ Holders of a commercial launchmaster certificate who can provide evidence of sea service within extreme limits will transition to a Skipper Coastal/Offshore certificate limited to their existing extreme limit. Holders of a commercial launchmaster certificate who can provide evidence of sea service aboard vessels of 24m or more within extended river limits will transition to an SRL certificate endorsed to 500GT. (Extreme limits and extended river limits are contained in the Shipping Restricted Limits Notice 1980.)

Using the legacy certificate

Legacy certificate holders who are transitioning to national and STCW-F certificates will be able to continue to work on New Zealand ships in the same capacity as they currently do, until 31 March 2019 when they will need to have transitioned to a new certificate.

However, legacy certificate holders who wish to exercise privileges on STCW/SOLAS ships will need to transition to the new 'equivalent' STCW certificate by 31 December 2016. If they haven't transitioned by that point, they will only be able to exercise the national related privileges that the legacy certificate provides.

These certificates will expire on 31 March 2019, after which they cannot be used in any capacity.

Specific transition issues

NZOM STCW-95 with Unit Standards 6912 and 6913

Holders of NZOM STCW-95 who have completed additional command modules (Unit Standards 6912 and 6913) will be able to apply for a Master <500GT certificate if they can show they have:

- sea service of no less than 24 months in charge of a navigational watch on a vessel of at least 24m or 80GT, 12 months of which must have been served within 10 years of application date and 12 months of which must have been served beyond restricted limits as chief mate or master while holding a certificate as Master <500GT Near-Coastal, or equivalent
- current ancillary proficiency certificates and meet the medical and eyesight standards required for the Master <500GT certificate
- passed a specific oral examination to test identified gaps in competency standards between the NZOM STCW-95 and Master <500GT certificate.

The privileges of the certificate will be restricted to being master of a vessel less than 500GT in any operating area. The holder must complete additional training and sea service to exercise the watchkeeping and near-coastal command privileges of the full Master 500GT unlimited certificate.

Marine engineer class 6

Existing holders of the MEC 6 certificate of competency who can show they have worked as a second engineer on fishing ships up to 3000kW in the unlimited area will be able to apply to have their certificate endorsed to continue to exercise this privilege. This only applies to seafarers who held a MEC 6 certificate prior to 1 April 2014.

Square-rigged sailing endorsement

A square-rigged endorsement can be issued to a seafarer even if he or she has not met the requirements of rule 32.65 up to 31 March 2019, if they have:

- completed 12 months' sea service in the last five years as a master on square-rigged sailing ships to which the endorsement is applied
- a safe operating record that is satisfactory to the Director.

The sea service must involve at least 24 months in charge of a navigational watch on a ship that is at least 24m or 80GT, 12 months of which must have been served within 10 years of the application date.

MEC 3 and Watchkeeper Deck

The new Part 32 includes transition arrangements for seafarers who start sea service or training for STCW-95 certificates before 1 April 2014. In this case, for MEC 3 (III/1) and Watchkeeper Deck (II/1) they will be able to:

- *either* continue to study and be awarded a STCW-95⁸ certificate up to 31 December 2016 (after which they will need to meet any additional requirements brought in by Manila⁹ when they renew their certificates)
- *or* transition to the equivalent STCW-10 certificate, in which case they will need to complete any additional courses or training to meet any additional requirements.

Where training providers have already incorporated modules into their existing courses to provide training in additional requirements to meet the STCW-10 standards, seafarers will be able to apply for the equivalent STCW-10 certificate.

Applications before 1 April 2014

If a seafarer has applied for a certificate or endorsement, but it has not been issued by 1 April 2014, he or she will be issued with the new corresponding certificate and/or endorsements.

Where there are different options for which certificate a seafarer may receive, MNZ will contact the applicant to advise him or her of the options available. The applicant has 30 days to inform MNZ of their preferred option. If, after 30 days, the applicant has not confirmed their preferred option, the Director will consider which of the available options is most appropriate on the basis of the training, sea service and any other information provided with the application.

Training or sea service started before 1 April 2014

Seafarers who start courses for national certificates before 1 April 2014 will be able to continue to study for the old certificate, but may apply for the equivalent new certificate.

They will need to meet the sea service requirements in full for either the old certificate or the new certificate. For example, seafarers who have completed the ILM training course will be able to apply for the new SRL certificate. They have the option of meeting the sea service requirements for the ILM (18 months) or for the new SRL certificate (200 hours plus completion of an approved training record book). In either case, they will need to pass the SRL final examination to receive the SRL certificate.

Training and examinations

Interim training courses and qualifications acceptable to the Director

New Zealand maritime qualifications and assessment standards associated with SeaCert certificates and endorsements are currently being developed as part of the NZQA Targeted Review of Qualifications (TROQ). New maritime qualifications, assessment standards and training courses are expected to be introduced by training providers during 2014.

Until these courses are available, Maritime New Zealand will continue to accept (up to January 2015) training courses that have been approved under the old Part 35 for the equivalent old certificates. These are detailed in the table below, which will be updated regularly as new qualifications, assessment standards and training courses are approved through the TROQ process.

Seafarers will still need to meet the sea service requirements, hold the appropriate ancillary proficiencies and pass the final examination for the new certificate.

8 STCW-95 certificate has been issued under STCW but not updated to take account of increased competency and proficiency requirements brought in when STCW was amended in 2010.

9 Amendments to STCW made in Manila in 2010.

SeaCert certificate	Acceptable training
Skipper Restricted Limits	ILM course
Skipper Restricted Limits Endorsed <24m	ILM course
Skipper Coastal/Offshore	NZOW with ILM endorsement course
Master Yacht <24m	No approved course
Marine Engineer Class 6	MEC 6 course
Marine Engineer Class 5	MEC 5
Marine Engineer Class 4	MEC 4
Deck Watch Rating	DWR
Engine Watch Rating	EWR
Able Seafarer Deck	AB D
Able Seafarer Engine	No approved course
Electro-technical Rating	No approved course
Watchkeeper Deck <500GT Near-Coastal	NZOW
Watchkeeper Deck	OICNW
Chief Mate <3000GT	No approved course
Chief Mate	Chief mate of foreign-going ship
Master <500GT (Coastal Limit)	NZOM
Master <500GT Near-Coastal	NZOM STCW-95
Master <500GT	No approved course
Master <3000GT	No approved course
Master	Master
Chief Mate Yacht	NZ chief mate (yacht)
Master Yacht <500GT	NZ master (yacht less than 500GT)
Master Yacht	NZ master (yacht)
Electro-technical Officer	No approved course
Marine Engineer Class 3	None – STCW-95 certificates for training started prior to 1 July 2013 ¹⁰

10 MEC 3 training courses are currently only instructing to STCW-95 standards. Maritime New Zealand will continue to issue STCW-95 certificates to MEC 3 for seafarers who began their training for MEC 3 prior to July 2013. These certificates will expire on 31 December 2016.

Holders of STCW-95 certificates will be eligible to renew their certificates and receive a STCW 10 certificate only if they meet the STCW 10 ancillary refresher requirements and can provide evidence that they have completed the required sea service while holding their STCW-95 MEC 3 or Watchkeeper Deck certificate (that is, sea service prior to receiving the certificate will not be taken into account), as per normal requirements.

Marine Engineer Class 2	MEC 2
Marine Engineer Class 2 Endorsed Chief Engineer	No approved course
Marine Engineer Class 1	MEC 1
GMDSS	GMDSS
Advanced Deckhand – Fishing	ADH with fishing endorsement
Mate Fishing Vessel – Limited	NZOW
Mate Fishing Vessel – Unlimited	Mate of deep-sea fishing vessel
Skipper Fishing Vessel – Limited	NZOM
Skipper Fishing Vessel – Unlimited	Master of deep-sea fishing vessel

Table 15 Training courses acceptable for new certificates

Industry-specific training

Approvals to conduct training for industry-specific training granted under the previous Part 35 are deemed to be an approval under the current Part 35.

The deemed approval will expire on the expiry date of the approval document or, if there is no expiry date indicated, on 31 March 2019, whichever occurs sooner. Until then, the organisation can continue to train for and issue certificates as long as it continues to fully comply with the terms and conditions of the original approval.

Industry-specific certificates issued under the old Part 35 approvals continue to remain valid, subject to any terms or conditions assigned to them.

Maritime examiners

People who hold a maritime document allowing them to act as a maritime examiner prior to 31 March 2014 are deemed to have been issued with an Examiner certificate when the new rule comes in. They will be able to continue to examine seafarers for new Part 32 certificates or endorsements that correspond to the certificates they are currently authorised to examine for.

Examiners who were approved under the previous Part 35 and have been deemed to hold an Examiner certificate will not be required to meet the currency requirements of Part 35 until after 31 March 2016.

The deemed Examiner certificate expires on the date the maritime document recognising the person as an examiner under the former Part 35 would have expired.

Medical requirements

Medical practitioners who were approved under the previous Part 34 can continue to examine seafarers until the approval expires on their maritime document, at which time they must renew their approval. The medical practitioner is able to examine seafarers seeking national seafarer certificates as a registered medical practitioner, whether or not the practitioner is approved under Part 34.

Minimum safe crewing

Seafarers can continue to use their existing certificate in the capacity of its identified equivalent certificate until it has been transitioned or renewed, subject to the transition provisions of Part 32 (no later than 31 March 2019). Some limitations apply to the use of STCW-95 certificates (or equivalents) after 31 December 2016, after which any vessel to which SOLAS/STCW applies must be crewed by seafarers who hold Manila-compliant certificates.

Ro-ro training

Seafarers who have current ro-ro passenger ship training are considered to have the equivalent of the ancillary proficiency in passenger safety, cargo safety and hull integrity. This expires on 31 December 2016, after which the seafarer will need to hold the ancillary proficiency if he or she has responsibility for embarking/disembarking passengers, loading/unloading/securing cargo and closing hull openings.

Minimum safe crewing documents

Minimum safe crewing documents (MSCD) for STCW/SOLAS ships will remain valid until their current expiry date or 31 December 2016, whichever is sooner. MSCD for other ships will remain valid until their expiry date or 31 March 2019, whichever is sooner.

Glossary

AB Deck	Able Seafarer Deck
AB Engine	Able Seafarer Engine
ADH	Advanced Deckhand
ADH-F	Advanced Deckhand – Fishing
Assigned inshore limit	A defined section of the coastal limits not beyond the limit of the territorial sea of New Zealand (and not more than 30NM from a safe haven), which a surveyor assigns to a ship as an inshore limit
Assistant engineer officer	A person under training to become an engineer officer
Candidate	A person who applies either to take an examination or to be issued with a certificate of competency, proficiency or endorsement
CDH	Certified deckhand
Certificate	Certificate of competency or certificate of proficiency
Certificate of competency	Corresponds to a maritime document that is described as a certificate of competency in Table 1 under Rule 32.02
Certificate of proficiency (CoP)	Corresponds to a maritime document that is described as a certificate of proficiency in Table 1 under Rule 32.02
Chief engineer officer	The senior engineer officer responsible for the mechanical propulsion and operation and maintenance of a ship's mechanical and electrical installations
Chief Mate	Chief Mate on ships of any gross tonnage in any operating area
Chief Mate <3000GT	Chief Mate on ships of less than 3000 gross tonnes in any operating area
Chief Mate Yacht	Chief Mate on superyachts of less than 3000GT in any operating area
Commercial ship	The same meaning as defined in Maritime Rule Part 32.02
Deck officer	An officer certificated in accordance with the provisions of Chapter II of STCW-78 as amended
DWR	Deck Watch Rating
EEZ	Exclusive economic zone –: the sea zone over which New Zealand has special rights for the exploration and use of marine resources, extending from the baseline of the territorial sea out to 200NM
Engineer officer	An officer certificated in accordance with the provisions of Regulation III/1, III/2 or III/3 of the STCW Convention
ETO	Electro-technical Officer
ETR	Electro-technical Rating
EWR	Engine Room Watch Rating
Fire fighting	Unit Standard 12310 or an equivalent revised standard

Fishing vessel	A vessel required to be registered under New Zealand fisheries legislation
GMDSS	Global maritime distress and safety system
GT	Gross tonnes/tonnage
IACS	International Association of Classification Societies, which supports safe ships and clean seas by providing technical support, compliance verification and research and development
ILM	Inshore Launchmaster
IMO	International Maritime Organization
Inshore limit	Inshore limits as described in Appendix 1 of Maritime Rule Part 20
IR	Integrated Rating
ISPS	International Ship and Port Facility Security Code, a set of measures to enhance the security of ships and port facilities
ITP	Industry training provider
Length	The same as load line length, as defined in Maritime Rule Part 47.2
Length overall	Overall length of a ship, as defined in detail in Maritime Rule Part 32.2
Limited waters	The combined area of the coastal limits and offshore limits
LLO	Local Launch Operator
Master	Master on ships of any gross tonnage in any operating area
Master <	Master on ships of less than a specified size in any operating area
Master <500GT	Master on ships of less than 500GT in any operating area
Master <500GT NC	Master on ships of less than 500GT in the near-coastal area
Master Yacht	Master on superyachts of less than 3000GT in any operating area
Master Yacht <24m NC	Master on sailing ships of less than 24m in the near-coastal area
Master Yacht <500GT	Master on superyachts of less than 500GT in any operating area
MCA	Maritime and Coastguard Agency (United Kingdom)
MEC 1	Marine Engineer Class 1
MEC 2	Marine Engineer Class 2
MEC 2 ECE	Marine Engineer Class 2 – endorsed as Chief Engineer
MEC 3	Marine Engineer Class 3
MEC 4	Marine Engineer Class 4
MEC 5	Marine Engineer Class 5
MEC 6	Marine Engineer Class 6

MFV	Mate of fishing vessels of less than 45m in length operating in limited waters
MFV-U	Mate of fishing vessels of any size operating in any operating area
MGOC	Maritime general operator's certificate
MGROC	Maritime general radiotelephone operator's certificate
Month	Equivalent to 30 days
MROC	Maritime restricted radiotelephone operator's certificate
MOSS	Maritime Operator Safety System
MSCD	Minimum safe crewing document
National award	A certificate or qualification issued by a training provider under the Education Act 1989
Near-coastal area	The combined area of the coastal limits and offshore limits
Near-coastal voyage	A voyage within the near-coastal area
NM	Nautical miles
NZOM	New Zealand offshore master
NZOW	New Zealand offshore watchkeeper
NZQA	New Zealand Qualifications Authority
NZQF	New Zealand Qualifications Framework
On-board yacht service	Time spent signed on a superyacht, regardless of vessel activity
Part 32	The maritime rule for seafarer certification
Part 35	The maritime rule for training and examinations. Section 2 allows for organisations to be approved to provide their own industry-specific training
Pleasure craft	The same meaning as defined in section 2 of the Maritime Transport Act
PTE	Private training establishment
QDC	Qualified Deck Crew
Record book	Record of sea service and vessels
Restricted limits	Enclosed water limits and inshore limits
RYA	Royal Yachting Association
Sailing ship	A ship that is designed to be navigated under wind power alone and for which any motor provided is an auxiliary means of propulsion
SCO	Skipper Coastal/Offshore
Sea service	Service on board a ship that is relevant to a certificate or endorsement
Sea service (superyachts)	A combination of sea service, stand-by service and/or yard service, as well as time spent at sea that may include time at anchor, or river or canal transits

	associated with a passage
Seagoing ship	As defined under Rule 32.02A
SFV	Master of fishing vessels of 24m metres or more but less than 45m length operating in limited waters as defined under rule 32.02
SFV-U	Master of fishing vessels of any length operating in any operating area
SRL	Skipper Restricted Limits
STCW-95	International Convention on Standards of Training, Certification and Watchkeeping for Seafarers, 1978, as amended in 1995
STCW	International Convention on Standards of Training, Certification and Watchkeeping for Seafarers, 1978, as amended in 2010
STCW-F	International Convention on Standards of Training, Certification and Watchkeeping for Fishing Vessel Personnel, 1995
Superyacht	A commercial yacht or sail training vessel of 24m or more loadline length and less than 3000GT that does not carry cargo or more than 12 passengers
Survival craft	Unit Standard 12309 or an equivalent revised standard
Territorial sea	New Zealand's territorial sea, as defined by section 3 of the Territorial Sea, Contiguous Zone and Exclusive Economic Zone Act 1977
Training record book	MNZ-approved book providing evidence of required competencies
TROQ	NZQA's Targeted Review of Qualifications
Voyage	A journey by water from one port either to another port, or back to the same port without calling at any other port
Warship	The same meaning as in section 2 of the Maritime Transport Act 1994
Watchkeeper Deck	Officer in charge of a navigational watch on ships of any gross tonnage in any operating area